



THE CITY OF NEW YORK
OFFICE OF THE MAYOR
NEW YORK, NY 10007

TECHNICAL MEMORANDUM 001

DOWNTOWN FAR ROCKAWAY REDEVELOPMENT PROJECT

CEQR No. 16DME010Q

ULURP Nos.: C170243(A) ZMQ, N170244(A) ZRQ, N170245 HGQ,
C170246 HUQ, C170247 HDQ, C170248 PPQ

April 26, 2017

A. INTRODUCTION

The City of New York, acting through the New York City (NYC) Economic Development Corporation (EDC), the New York City Department of Housing Preservation and Development (HPD), and the New York City Department of Citywide Administrative Services (DCAS), is proposing a series of land use actions (C170243 ZMQ, N170244 ZRQ, N170245 HGQ, C170246 HUQ, C170247 HDQ, and C170248 PPQ), including zoning map amendments, zoning text amendments, disposition and acquisition of property, and the designation and approval of an Urban Renewal Area (URA) and Plan (URP) to implement recommendations of a comprehensive plan to redevelop and revitalize the Downtown Far Rockaway neighborhood of Queens, Community District 14. Known as the Downtown Far Rockaway Redevelopment Project, the Proposed Actions are intended to transform underutilized sites with mixed-use, transit-oriented development, and unlock the potential for development throughout Downtown Far Rockaway. The Proposed Actions would allow new and more dense residential uses in locations where zoning does not permit them today, and a mix of commercial and community facility uses that would complement the downtown setting and the public realm. New commercial and community facility spaces would occupy the ground floor and lower floors of new mixed-use buildings. The Proposed Actions would concentrate density outside of the existing flood zones and near mass transit, while blending new development into the existing neighborhood fabric.

The Office of the Deputy Mayor for Housing and Economic Development (ODMHED), serving as lead agency, oversaw the preparation of a Draft Environmental Impact Statement (DEIS) in conformance with 2014 *City Environmental Quality Review (CEQR) Technical Manual* guidelines. The lead agency conducted a coordinated review of the Proposed Actions with

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Involved Agencies, which include the NYC Departments of City Planning (DCP), HPD, and DCAS. In addition, several agencies have participated in the environmental review as Interested Agencies under CEQR, including the NYC Department of Transportation (DOT), the NYC Department of Sanitation (DSNY), the NYC Department of Environmental Protection (DEP), NYC Transit (NYCT), and the Metropolitan Transit Authority (MTA). The DEIS for the Proposed Actions was accepted as complete on January 27th, 2017. A public hearing on the DEIS will be held at a later date to be announced. Advance notice will be given of the time and place of the hearing. The corresponding Uniform Land Use Review Procedure (ULURP) application was certified on January 30, 2017.

In conjunction with this technical memorandum, the City of New York has prepared and filed an amended ULURP application (referred to hereafter as the “A-Application”) that addresses issues raised just before or shortly after issuance of the DEIS. The A-Application consists of a series of modifications to the Proposed Actions that require additional environmental analysis, which is the focus of this technical memorandum.¹ These modifications include zoning text amendments and zoning map amendments, as described below. These modifications were crafted in response to feedback on the application, to allow for additional development projects that meet the purpose and need for the Proposed Actions, and to ensure successful site planning on a complex and irregular site.

1. The modified zoning text amendments (N170244(A) ZRQ) would improve design flexibility within Subdistrict A of the proposed Special Downtown Far Rockaway District (“Special District”) to promote better site planning on the current Far Rockaway Shopping Center, a key future development site. Subdistrict A is mainly comprised of the Shopping Center site, a large but highly irregular underutilized site in the heart of Downtown Far Rockaway and central to the Proposed Downtown Far Rockaway Urban Renewal Area (DFRURA).
2. The modified zoning map amendments (C170243(A) ZMQ) would slightly extend the Rezoning Area proposed in the DEIS (“DEIS Rezoning Area”) in two places and extend a proposed C2-4 commercial overlay to a site already located within the DEIS Rezoning Area (see **Figures 1 and 2**).

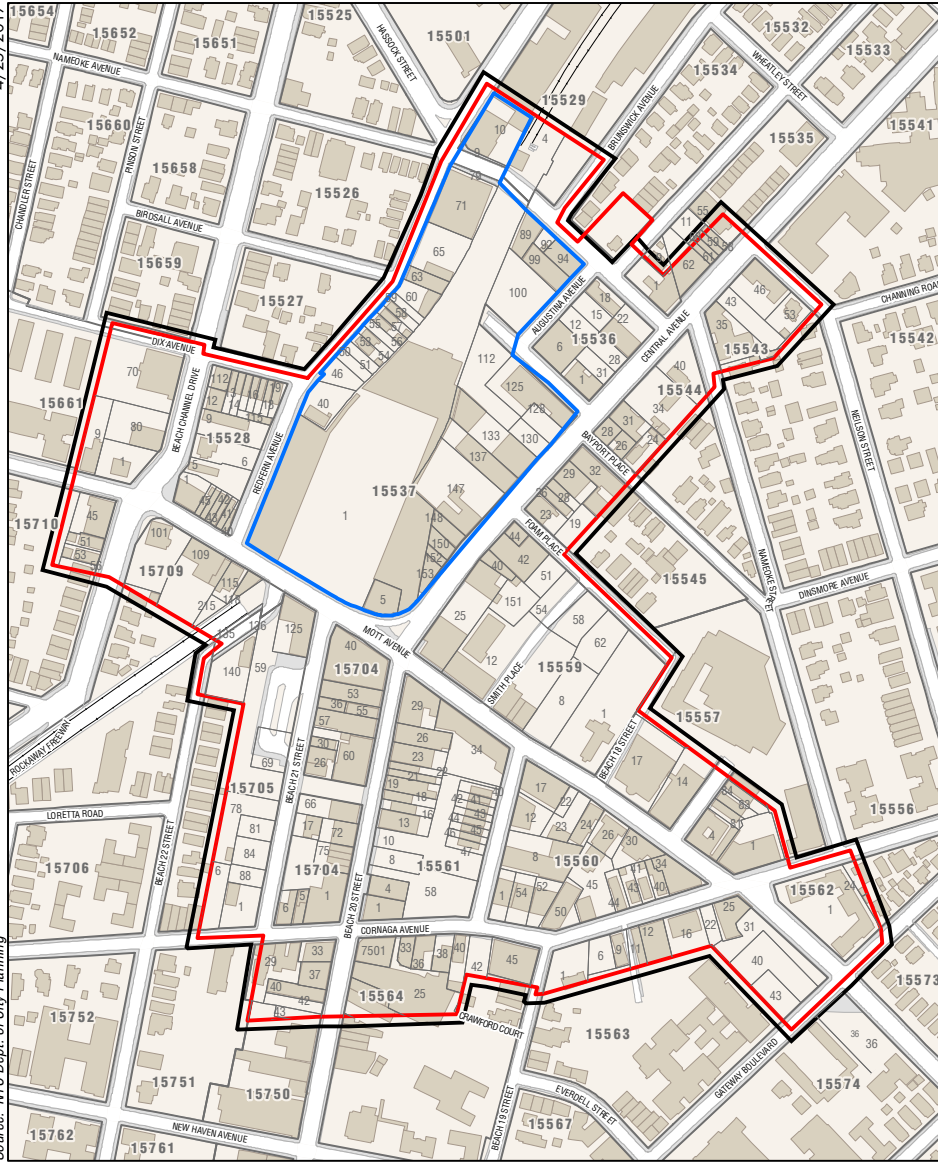
In addition to the modifications described above, the A-Application includes a zoning text amendment to introduce a new authorization available to developments within the Special District. This authorization would allow for the modification of height and setback regulations, yard regulations, and regulations governing the minimum required distance between buildings and minimum required distance between legally required windows and walls or lot lines. In order to receive authorization to modify these bulk regulations, a development would need to demonstrate to the City Planning Commission (CPC) that such modifications would provide a better distribution of bulk on the zoning lot, resulting in a superior site plan; would not unduly increase the bulk of buildings; and would not create traffic congestion in the surrounding area. In addition, the CPC could prescribe appropriate conditions and safeguards to minimize adverse effects on the character of the surrounding area.

This technical memorandum presents revised analyses of the Proposed Actions based on the A-Application (see **Appendix A** for the full revised Special District text). It considers whether the

¹ Specifically the amended zoning text and zoning map modifies the ULURP application C170243 ZMQ and N170244 ZRQ, but does not modify N170245 HGQ, C170246 HUQ, C170247 HDQ, or C170248 PPQ.

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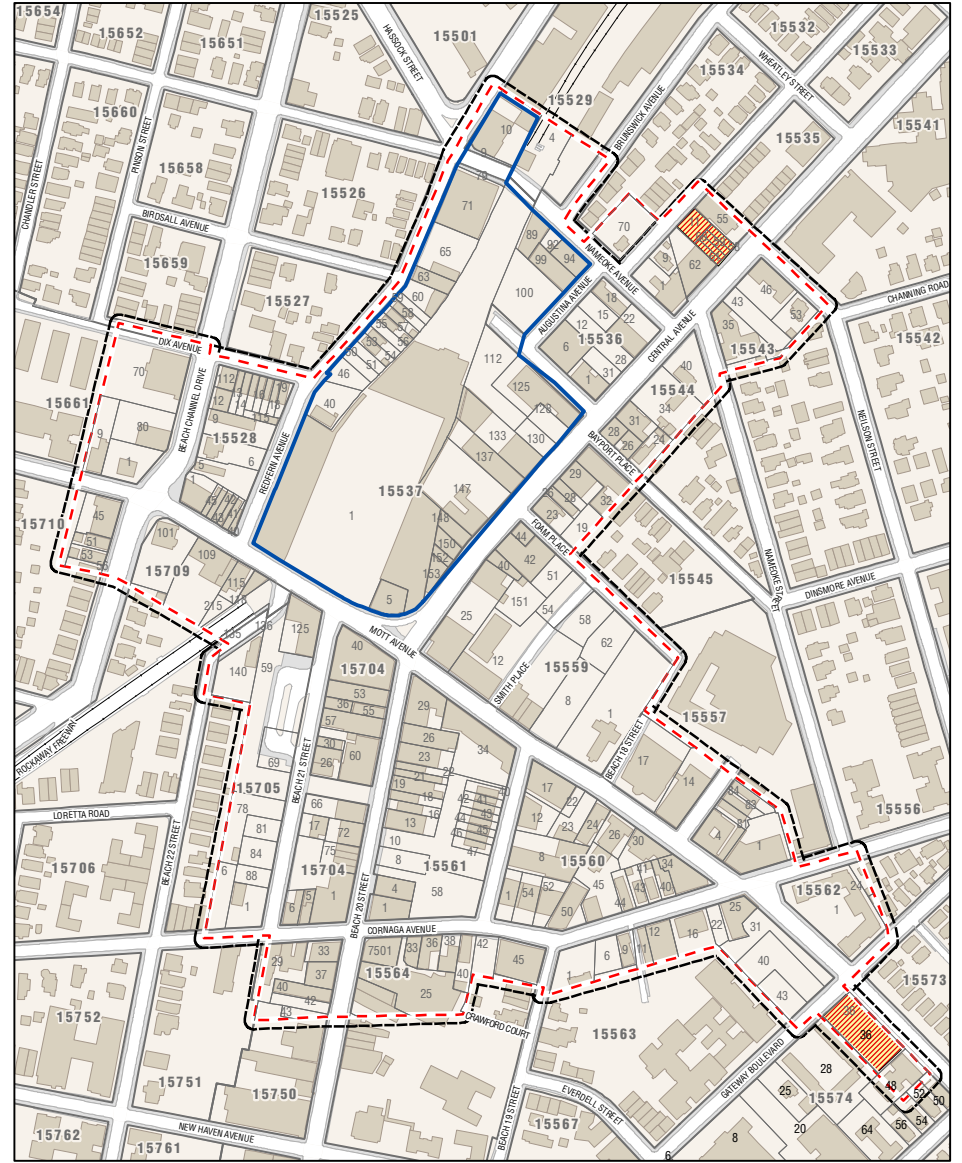
Source: NYC Dept. of City Planning



DEIS Proposed Rezoning

- DEIS Project Area
- DEIS Rezoning Area
- Subdistrict A

Downtown Far Rockaway Redevelopment Project



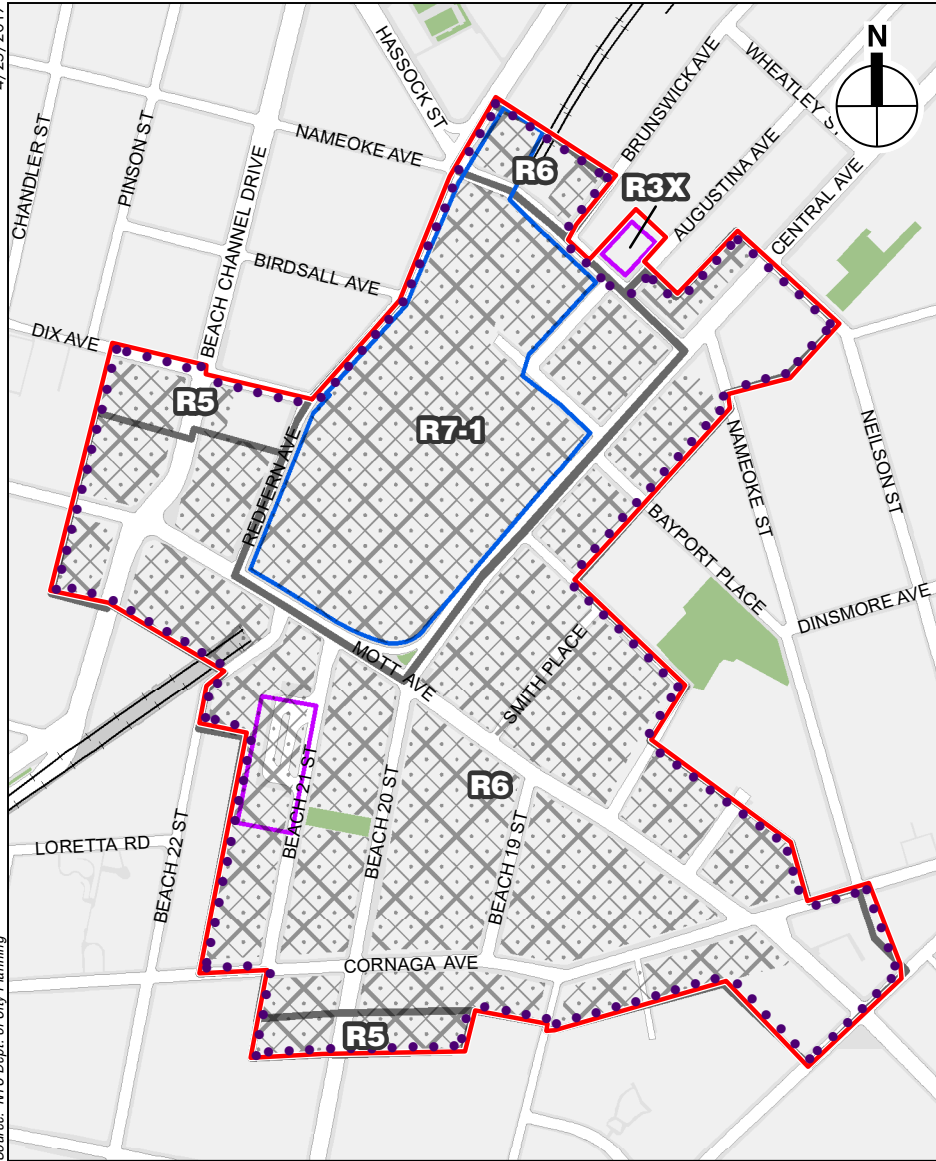
A-Application Proposed Rezoning

- A-Application Rezoning Area/Special DTFR District
- A-Application Project Area
- Subdistrict A
- A-Application Projected Development Sites







A-Application
Amended Project Area
Figure 1

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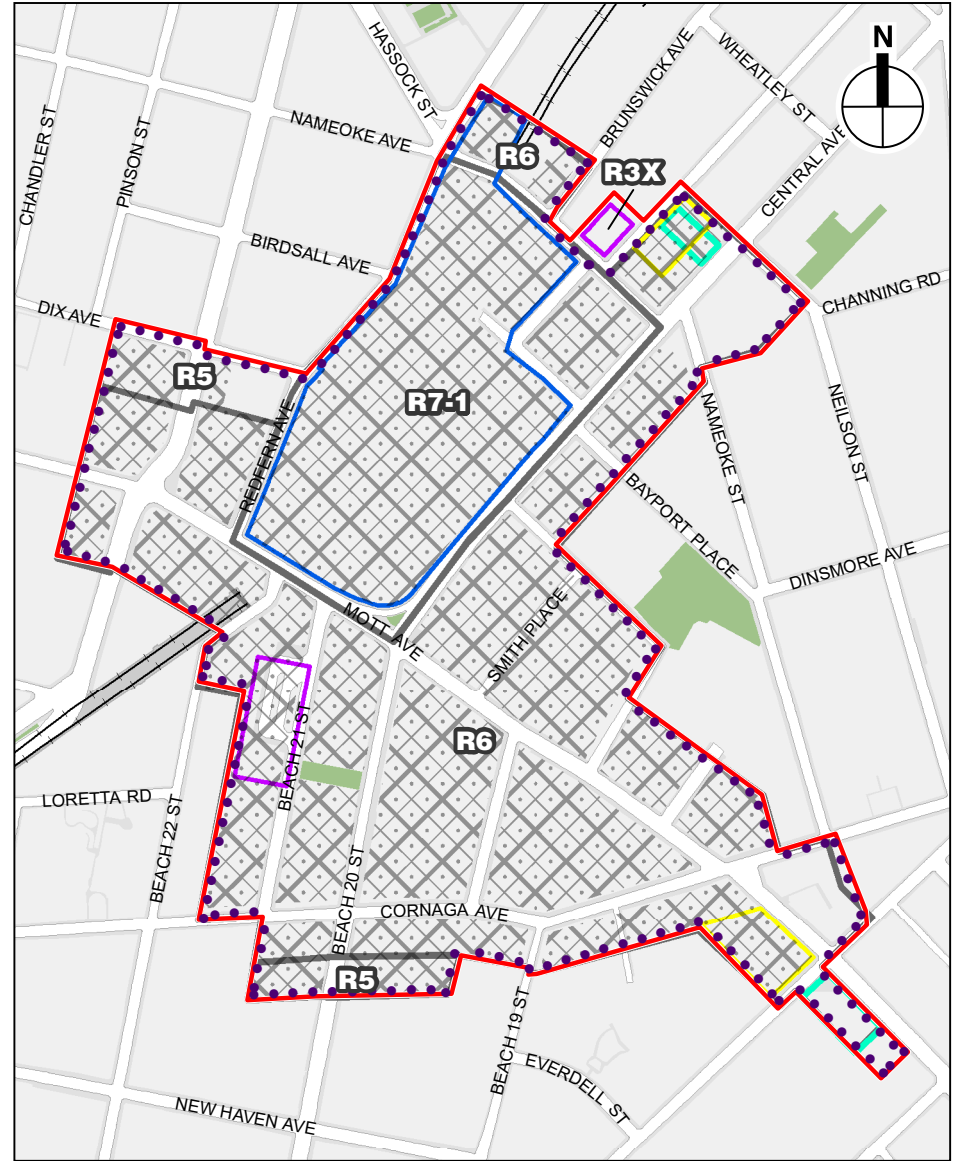
Source: NYC Dept. of City Planning






DEIS Proposed Rezoning

-  DEIS Rezoning Area
-  Subdistrict A
-  DEIS Disposition Sites
-  DEIS Project Area
-  Proposed C2-4 Commercial Overlay District
-  Zoning Districts

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A-Application Proposed Rezoning

-  A-Application Projected Development Sites
-  A-Application Project Area
-  A-Application Proposed C2-4 Commercial Overlay District
-  Subdistrict A
-  A-Application Rezoning Area/Special DTFR District
-  Disposition Sites

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A-Application Amended Rezoning Actions
Figure 2

A-Application would result in any new or different significant adverse environmental impacts that were not already identified in the DEIS. In order to assess the potential impacts of the new authorization introduced under the A-Application, this technical memorandum also includes for applicable technical areas² a conceptual analysis of potential modifications to the design of one of the Projected Development Sites analyzed in the DEIS (Projected Development Site 9). Finally, the transit analyses within the A-Application have been updated based on a comment raised by MTA during the DEIS public review process about ridership levels on the Q113/Q114. Specifically, the assumption about the percentage of riders boarding or alighting in Far Rockaway and travelling to and from Downtown Jamaica has been updated.

As set forth below, this technical memorandum concludes that the Proposed Actions with the modifications described in the A-Application would not result in any new or different significant adverse impacts that were not already identified in the DEIS; however, it would worsen certain significant adverse impacts identified in the DEIS related to publicly funded child care centers, open space, traffic, transit, and construction noise. To provide a complete consideration of the proposed modifications described in the A-Application and the likely effects resulting from it, each DEIS technical analysis area is considered.

The analysis conducted in this technical memorandum will also be incorporated in the Final EIS (FEIS).

B. DESCRIPTION OF THE AMENDED APPLICATION

The A-Application includes text amendments as well as an extension of the DEIS Rezoning Area proposed in the DEIS. The text amendments would promote better site planning within Subdistrict A of the proposed Special Downtown Far Rockaway District (“Special District”) and would respond to feedback heard during the public review process. The proposed extension of the DEIS Rezoning Area would cover areas immediately proximate to the DEIS Rezoning Area boundary, and would allow for the development of projects that further the purpose and need for the Proposed Actions.

ZONING TEXT AMENDMENTS

The A-Application text amendments would primarily focus on the portions of the regulations governing Subdistrict A (Zoning Resolution [ZR] Section 136-30) and would affect the following portions of the proposed zoning text:

- *Street wall location*

² The zoning text amendment authorizing modifications to bulk regulations would be a CPC discretionary action. CPC findings would require that such modifications would not unduly increase the bulk of the building, density of population, or intensity of use in any block to the detriment of the occupants of buildings in the block or nearby blocks. Therefore, as compared to the findings in the DEIS, the Authorization would not have the potential to result in new or different impacts with respect to: Land Use, Zoning, and Public Policy; Socioeconomic Conditions; Community Facilities and Services; Open Space; Historic Resources; Natural Resources; Hazardous Materials; Water and Sewer Infrastructure; Solid Waste and Sanitation Services; Energy; Transportation; Air Quality; Greenhouse Gas Emissions and Resiliency; Noise; Public Health; or Construction Impacts. This technical memorandum considers the potential effects of the authorization on: Shadows; Urban Design and Visual Resources; and Neighborhood Character.

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The aggregate width of a street wall required to be within 8 feet of the street line would be reduced from 70 percent to 40 percent on blocks less than 100 wide between parallel streets. For portions of buildings or building segments with frontage on Redfern Avenue located between the prolongation of the northerly street line of Dix Avenue and a line 150 feet south of and parallel to Nameoke Street, the street wall location rules of ZR Section 136-221 would not apply and instead portions of ZR Section 23-661 would apply.

- *Street wall recesses*

Street wall recesses would be allowed to be located within 30 feet of Redfern Avenue, except at the intersection of Redfern Avenue and Mott Avenue, and the minimum depth of any street wall recess would be reduced from 8 feet to 3 feet.

- *Minimum and Maximum Base Heights*

The maximum base height for portions of buildings fronting on, or within 100 feet of a street (other than Redfern Avenue) would be reduced from 70 feet to 65 feet. In areas fronting on a private street or a publicly accessible open space and beyond 100 feet of a street that is not a private street or publicly accessible space, the maximum base height would be reduced from 90 feet to 85 feet. The reduced height would allow for base heights along Mott Avenue that would match the surrounding context and heights.

- *Maximum Building Height and Horizontal Dimension for Tall Buildings*

The area where towers could potentially land would be reduced, further pushing towers to the interior of the Subdistrict. Maximum building heights along Mott Avenue, Nameoke Avenue, and Redfern Avenue would also be established.

- *Maximum length of buildings*

For portions of buildings that are not located directly below tower portions, the outermost walls of each story located entirely above a height of nine stories or 95 feet, whichever is less, shall be inscribed within a rectangle with a maximum length of any side being 170 feet.

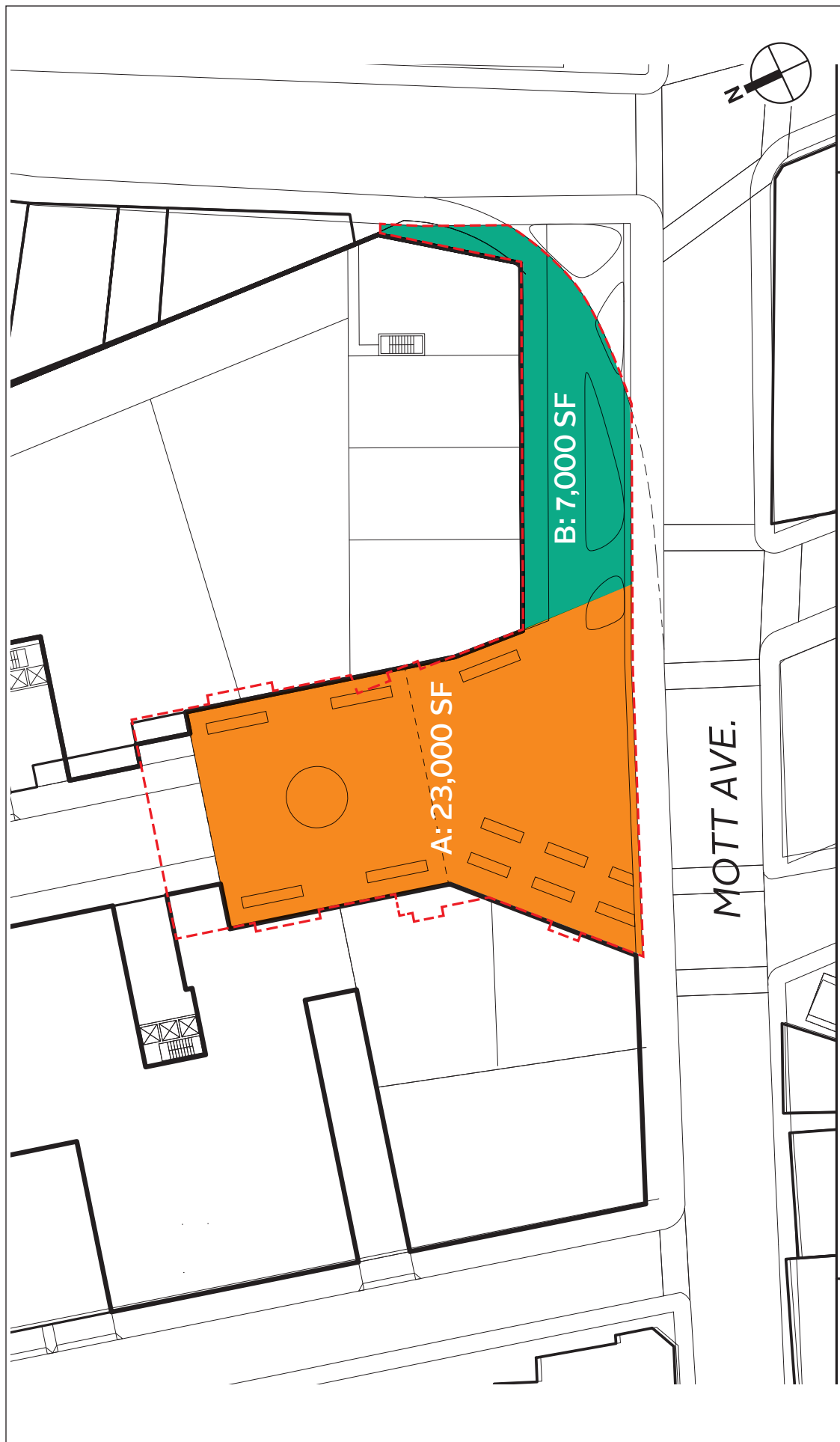
- *Certification*

The Chairpersons Certification and private street design requirements would be amended to allow for flexibility for non- DOT standard curb-to-curb and sidewalk widths with DOT Commissioner approval.

- *Publicly accessible open space requirements*

In order to better align public open space design to support active retail space, the amount of publicly accessible open space required would be adjusted. The minimum area within Open Area A would be reduced from 27,000 square feet to 23,000 square feet, and the required minimum area within Open Area B would be reduced from 7,500 square feet to 7,000 square feet (**Figure 3** shows the adjusted open space plan). In addition, Open Area A's shape and dimensions would be adjusted to promote flexibility to respond to the site's unique characteristics. The regulations would also be adjusted to allow a kiosk of up to 400 square feet within Open Area A. Planting requirements, the limitations on the amount of accessory signage permitted on establishments fronting on an open area, and the maximum width of a residential lobby adjacent to open areas would be relaxed.

As described above, the A-Application would include an authorization that would allow the CPC to authorize modifications of bulk regulations. This authorization would provide additional flexibility for developing the irregular lots within the Special District, provided that the modifications result in a superior site plan; do not exceed the maximum permitted building



A-Application Amended Open Space Plan
Figure 3

heights and horizontal dimensions for tall buildings; do not unduly increase the bulk of buildings or unduly obstruct access of adequate light and air to the detriment of the occupants or users of buildings on the block or nearby blocks, or of people using the public streets and other public spaces; and would not create traffic congestion. This technical memorandum includes a conceptual analysis of the potential environment effects of the authorization to modify bulk regulations based on a representative site within the Rezoning Area. While it is not known which sites may apply for the proposed authorization, for the purposes of this analysis it is assumed that Projected Development Site 9 would be developed in accordance with the authorization. This Projected Development Site was chosen based on the site's characteristics and its location. By utilizing the authorization, this Projected Development Site could be developed with a slightly shorter building that is more in context with the adjacent buildings, rather than a taller building.

Finally, additional measures proposed in the A-Application would promote design flexibility within Subdistrict A, and would include: applying ground floor use regulations to buildings within a certain distance of Mott Avenue and fronting on open area; increasing the maximum width of a residential lobby adjacent to open areas; applying transparency and parking wrap requirements to building frontages along the proposed open space; adjusting dormer regulations; and providing an additional degree of flexibility for lot coverage requirements in R6 districts when the Mandatory Inclusionary Housing (MIH) program is applied.

ZONING MAP AMENDMENTS

The A-Application includes amendments to the zoning map to extend the boundary of the Rezoning Area in two places (see **Figures 1 and 2**). The zoning map modifications include extending the proposed C2-4 commercial overlay southeast along Mott Avenue at a depth of 100 feet to portions of Lots 31, 40 and 43 on Block 15563. The certified proposal would have removed the existing C2-2 commercial overlay from a proposed R6 district on these lots. Within the DEIS this site was identified as Projected Development Site 9. Extending the C2-4 commercial overlay would allow for this site to be developed with a mixed residential and commercial development with a commercial use on the ground floor where a community facility use had initially been projected in the DEIS.

In addition, the Rezoning Area, the proposed R6 residential district, and the proposed Special District would be extended across Gateway Boulevard to cover Lots part of 28, 36, and part of 48 on Block 15574 to facilitate an affordable housing development on Lot 36 with publically accessible open space as well as private open space for residents. The Rezoning Area would extend approximately 10 feet into Lot 28, which would not be a sufficient depth for the R6 zoning to be utilized on Lot 28. The Rezoning Area would also extend 100 feet into Lot 48. Extending the R6 onto a portion of Lot 48 would help bring an existing six-story, 43-unit multi-family building located on this site into compliance with zoning. Current R5 zoning limits the maximum height of a building to four stories.

Finally, the zoning map amendments would extend the Rezoning Area, the proposed R6/C2-4 districts, and the proposed Special District to cover the Augustina Avenue facing portion of Block 15535, south of a street center line prolongation of Neilson Street. The Central Avenue portion of Block 15535 is currently within the Rezoning Area and the Augustina Avenue portion is not. The affected lots include Lots 9, 11, part of 62, and part of 55. The extension of the Rezoning Area would support the development on Lots 11, 58, 59, 60 and 61 of a mixed-use building with a grocery store and laundromat on the ground floor and affordable housing above.

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While Lots 9, 55, and 62 would be affected by this extension, they are developed either with a house of worship and a school operated by a house of worship, or under common ownership with one of those uses. As such, these additional lots are not expected to be redeveloped based on the criteria for development sites as defined in the DEIS.

C. A-APPLICATION REASONABLE WORST-CASE DEVELOPMENT SCENARIO

The A-Application would result in some changes to the Reasonable Worst-Case Development Scenario (RWCDS) presented in the DEIS and accordingly, a modified Future with the Proposed Actions Condition was created (referred to hereafter as the “Amended With-Action Condition” or “Amended RWCDS”). With the proposed A-Application Rezoning Area, there would be two new Projected Development Sites in the Amended RWCDS—Projected Development Sites 18 and 19 (see **Table 1** and **Figure 4**).

Table 1
Additional Projected Development Sites with the Amended With-Action Condition

Development Site	Block No.	Affected Lots
Projected Development Site 18	15574	36
Projected Development Site 19	15535	11, 58, 59, 60, 61

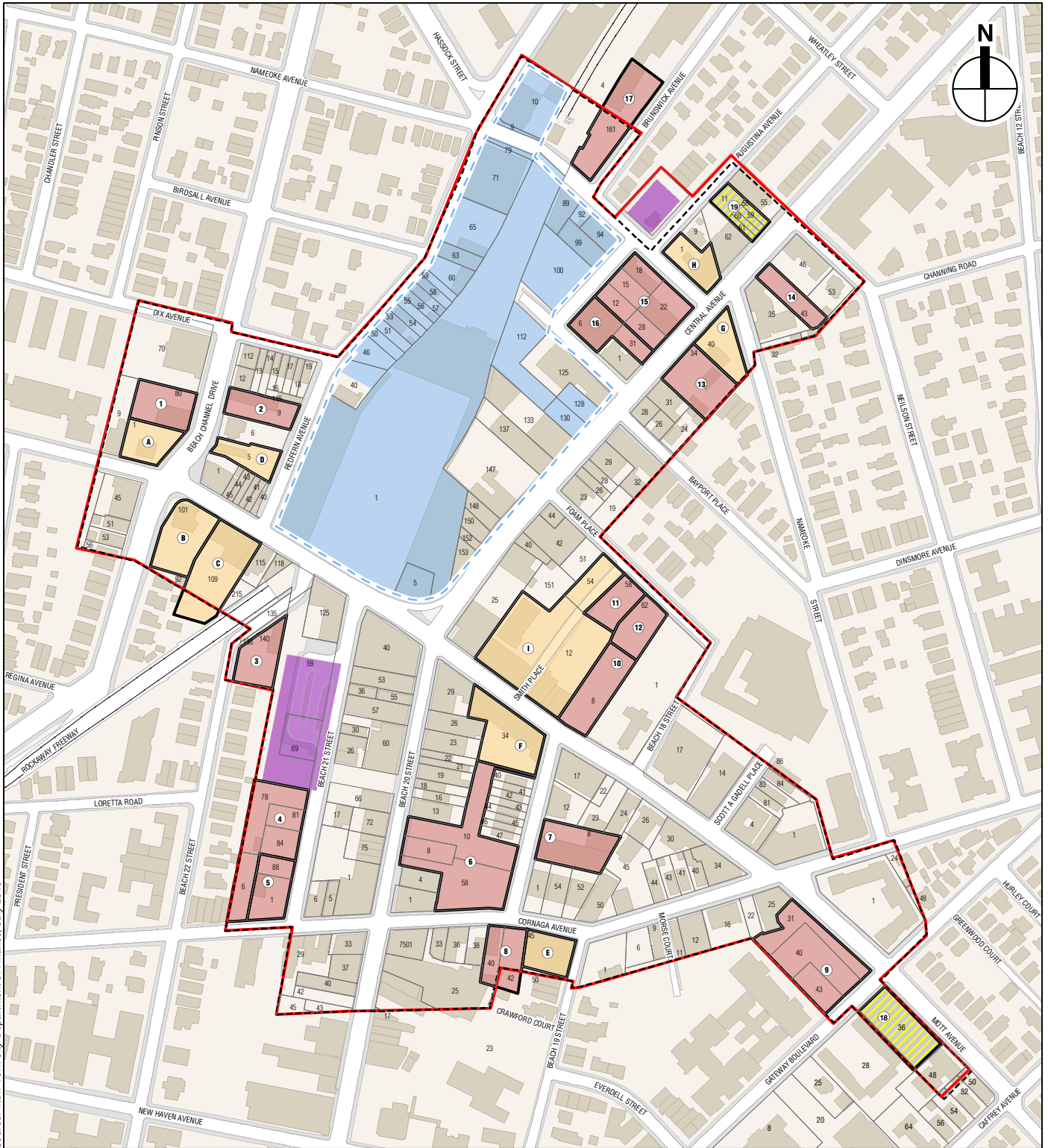
Tables 2 and 3 detail the RWCDS program assumptions for Projected Development Sites 18 and 19, respectively. **Figures 5 and 6** show illustrative massing for Projected Development Sites 18 and 19, respectively. With respect to Projected Development Site 18, the developer intends to include an estimated 6,432-square-foot (sf) publicly accessible active open space as part of the Site 18 development program as well as private open space for residents. However, the provision of publicly accessible open space would not be required, and therefore is not accounted for in the Amended RWCDS for this analysis.

Table 2
Additional Projected Development Site 18 RWCDS Program Assumptions

Use	Existing and No Action Conditions¹	With-Action Condition	Increment
<i>Residential Units (DUs)</i>	0	71	71
Retail (GSF)	0	0	0
Community Facility (GSF)	0	0	0

Notes: 1. Consistent with RWCDS analysis framework used in the DEIS, in the future without the Proposed Actions (the No Action Condition), the Projected Development Sites are assumed to remain unchanged from existing conditions.

In addition, in the Amended With-Action condition, the C2-4 overlay would be extended along Mott Avenue to cover the entire block face between Cornaga Avenue and Gateway Boulevard. This would change the RWCDS program assumptions for Projected Development Site 9, as shown in **Table 4**.



- DEIS Proposed DFRURA
- DEIS Disposition Sites
- DEIS Projected Development Sites
- DEIS Potential Development Sites
- A-Application Project Area
- A-Application Rezoning Area
- A-Application Projected Development Sites
- Subdistrict A

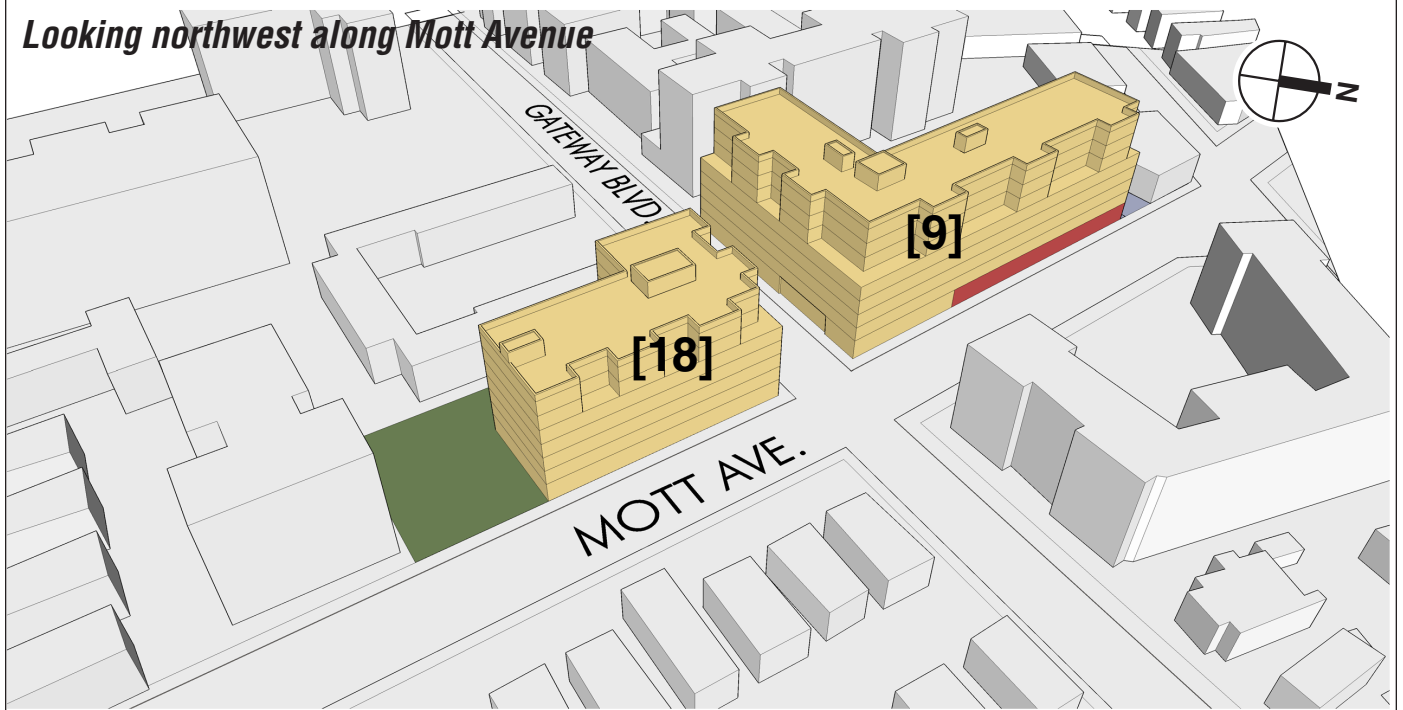
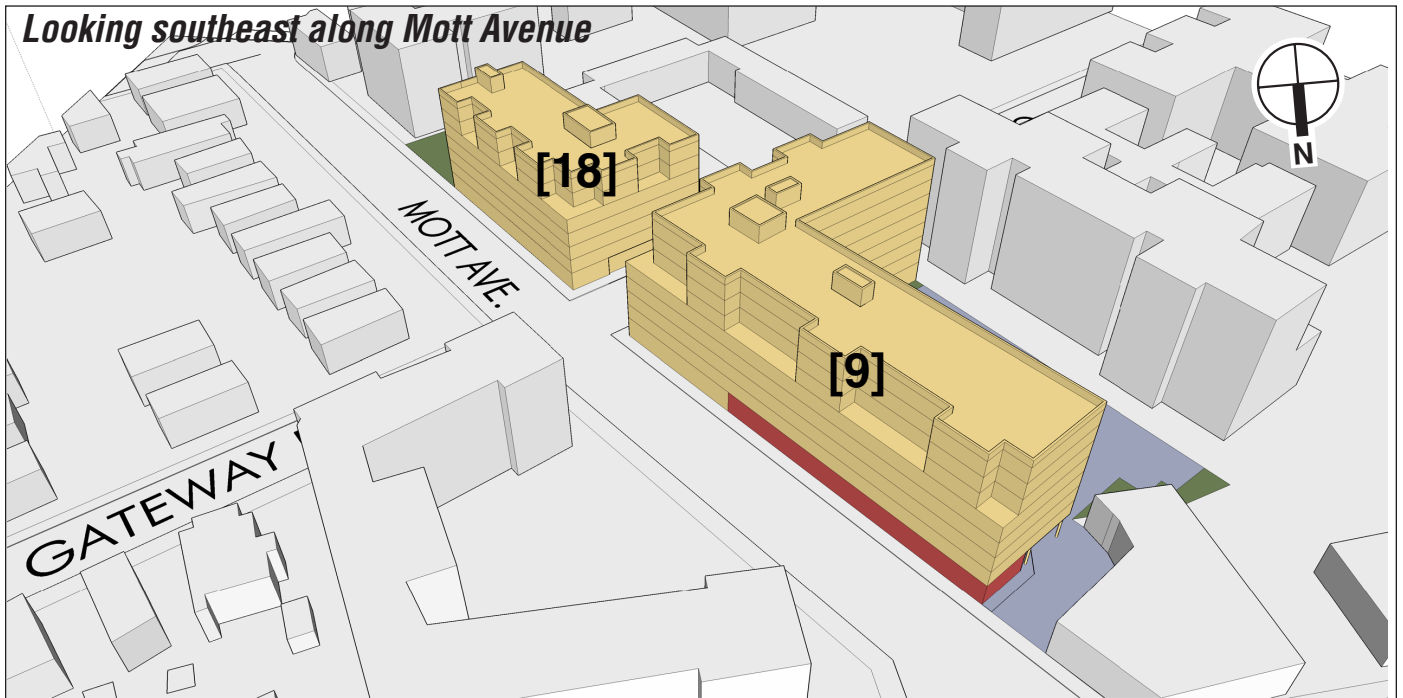
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Note: Projected/Potential Site boundaries illustrate the extent of the entire tax lots that would contribute to total development area. Actual projected/potential development footprints may differ.

Disposition Sites:
 Block 15705, Lots 59 and 69 are the DOT/MTA Disposition Site
 Block 15534, Lot 70 is the DSNY Disposition Site

A-Application Amended
 Project Area Components
Figure 4

Downtown Far Rockaway Redevelopment Project

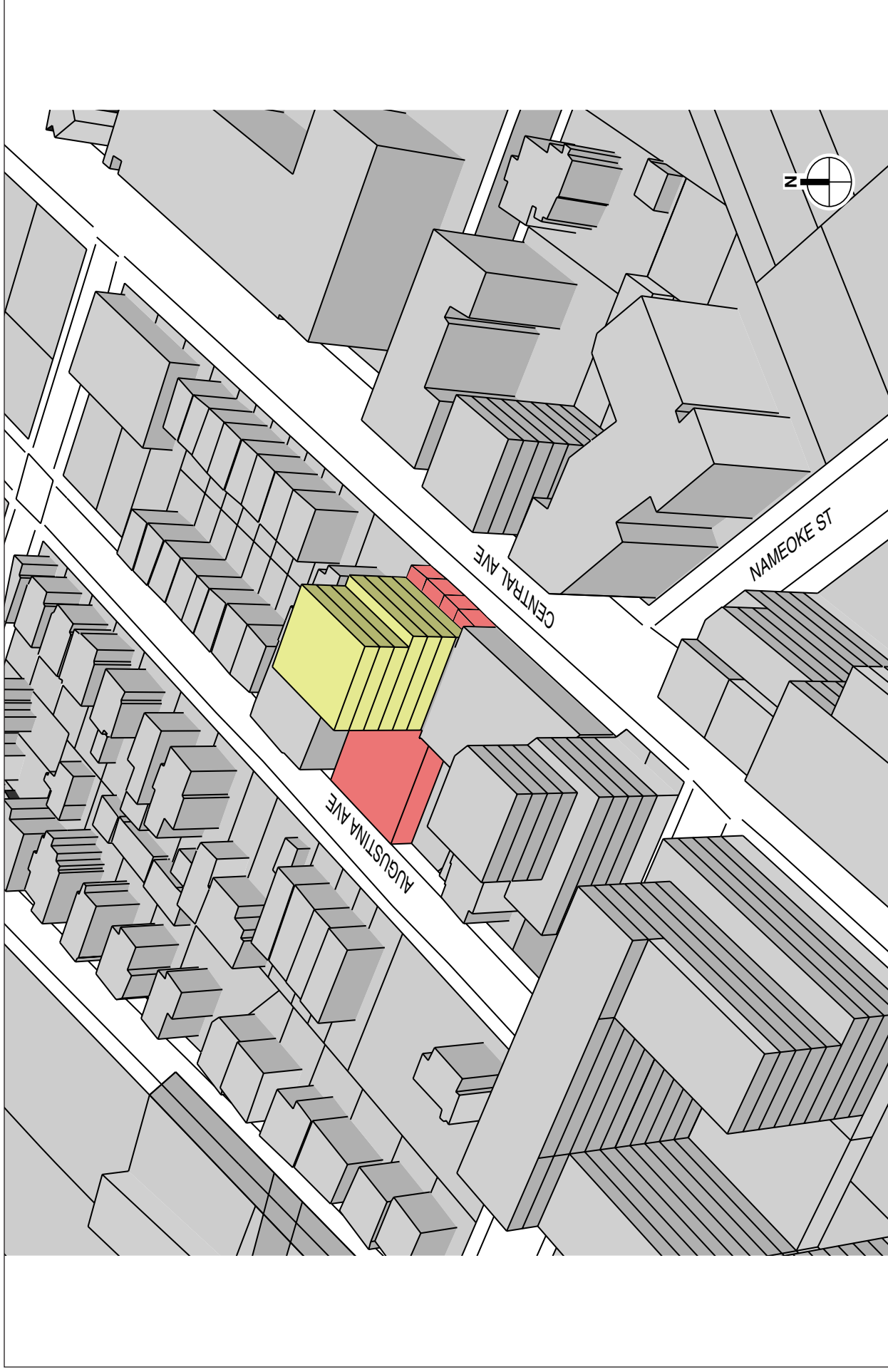


Source: Related

Projected Development Site 9, as compared to the community facility ground-floor use analyzed in the DEIS, the Amended RWCDs now includes a ground-floor retail use instead of a community facility use.

Projected Development Site 18 is not analyzed in the DEIS, and is introduced by the Amended RWCDs.

Projected Development Site 9 and
Projected Development Site 18
Illustrative Massing
Figure 5



This is an illustrative massing showing the Amended With-Action condition in 2032

Table 3
Additional Projected Development Site 19 RWCDs Program Assumptions

Use	Existing and No Action Conditions ¹	With-Action Condition	Increment
<i>Residential Units (DUs)</i>	0	27	27
Retail (GSF)	4,160	7,820	3,660
Community Facility (GSF)	0	0	0
Publicly Accessible Open Space (SF)	0	0	0

Notes: 1. Consistent with RWCDs analysis framework used in the DEIS, in the future without the Proposed Actions (the No Action Condition), the Projected Development Sites are assumed to remain unchanged from existing conditions.

Table 4
Amended Projected Development Site 9 RWCDs Assumption

Use	Proposed Actions Increment (from DEIS)	A-Application Amendment Increment	Difference
<i>Residential Units (DUs)</i>	135	133	-2
Retail (GSF)	0	8,000	8,000
Community Facility (GSF)	6,000	0	-6,000
Publicly Accessible Open Space (SF)	0	0	0

As compared to the RWCDs presented in the DEIS, the Amended With-Action Condition would result in a 96-DU increase, an 11,660-gsf increase in the retail increment, a 6,000-gsf decrease in the community facility increment, and a 5,669-sf decrease in the amount of publicly accessible open space (from the reduction of plaza space on the Proposed DFRURA, and conservatively assuming no new publicly accessible open space at Projected Development Site 18). **Table 5** presents the Amended RWCDs program assumptions that are used to study the potential effects of the A-Application in this technical memorandum. Detailed Amended RWCDs tables are provided in **Appendix B**.

Table 5
Difference in Proposed Actions Increment vs A-Application Increment Amended RWCDs

Use	Proposed Actions Increment (from DEIS)	A-Application Amendment Increment	Difference
<i>Residential Units (DUs)</i>	3,027	3,123	96
Retail (GSF)	152,935	164,595	11,660
Community Facility (GSF)	86,947	80,947	-6,000
Publicly Accessible Open Space (SF)	35,669	30,000	-5,669

In addition to the RWCDs program changes described above, the proposed zoning text amendments affecting Subdistrict A of the Special District would result in adjustments to allowable building massing within the Proposed DFRURA. Although the overall program on the Proposed DFRURA would not change, the proposed text amendments would result in the following adjustments to massing within the Proposed DFRURA (illustrated in **Figures 7 and 8**):

- **Building B:** Along Mott Avenue, the base of the building would be reduced to five stories before a setback, rising to a total of eight stories along Mott Avenue. The overall height of the building would be reduced from 12 to 11 stories.



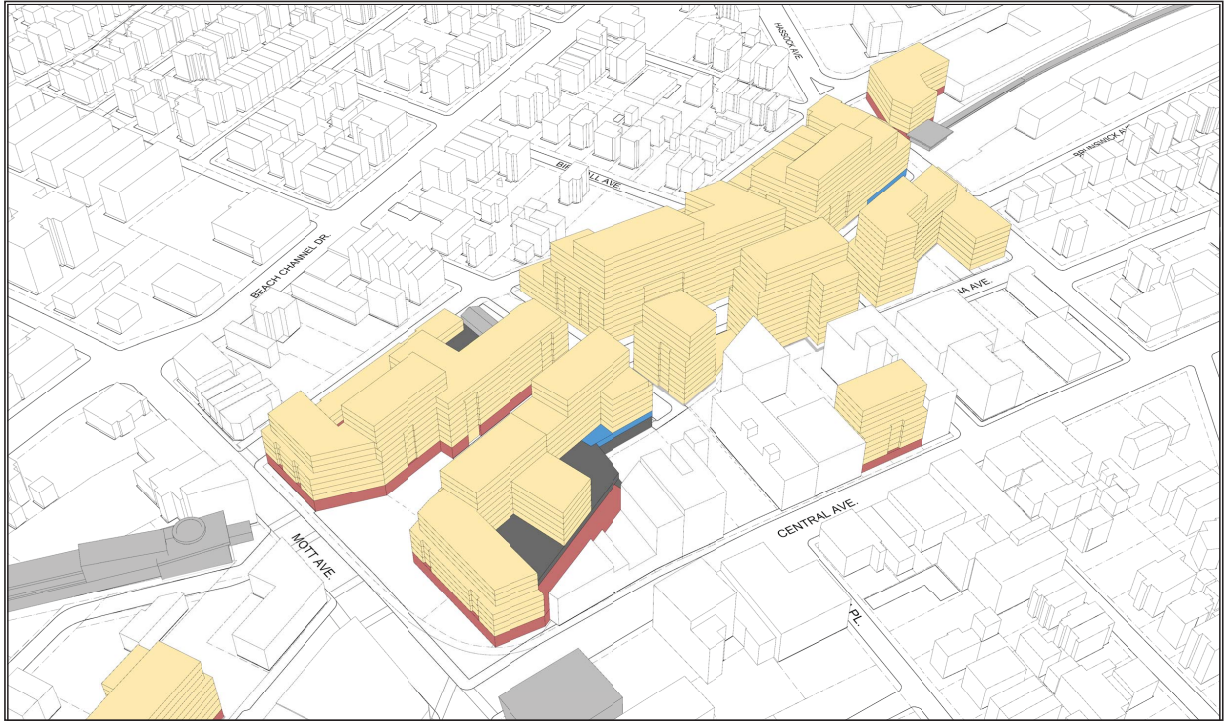
Looking North



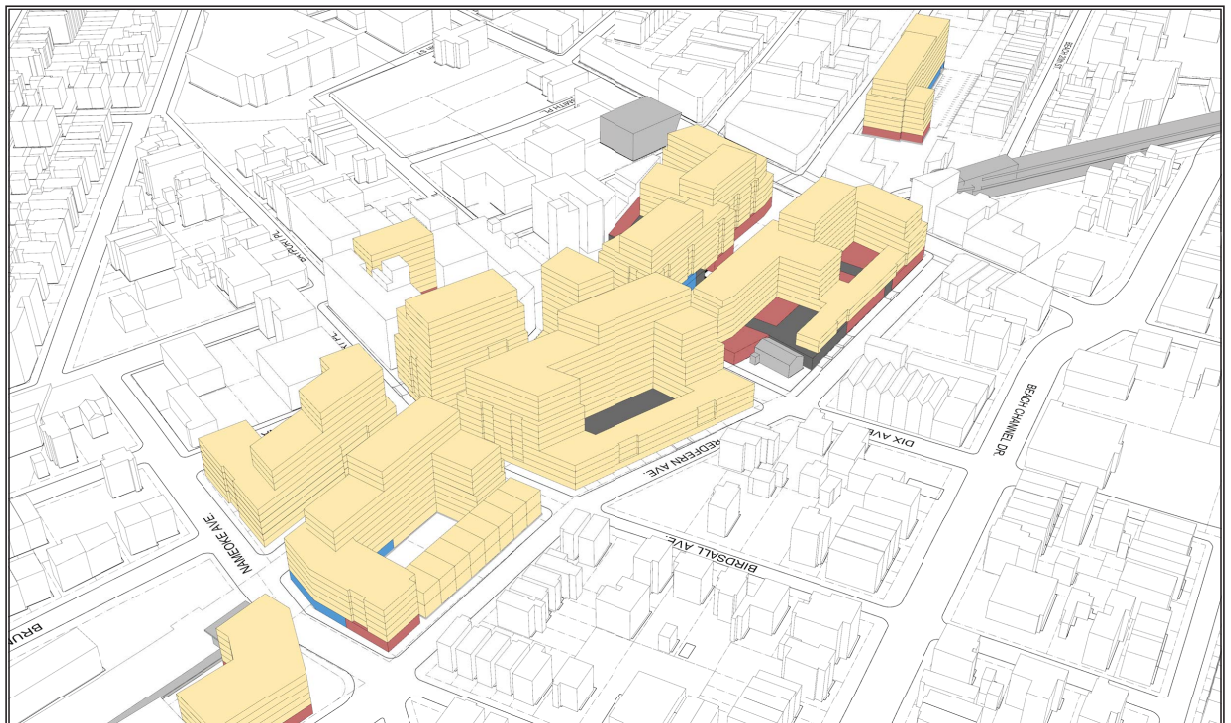
Looking South

- Community Facility Use
- Commercial (Retail) Use
- Residential Use
- Parking

DEIS Proposed DFRURA Illustrative Rendering with Maximum Building Envelopes

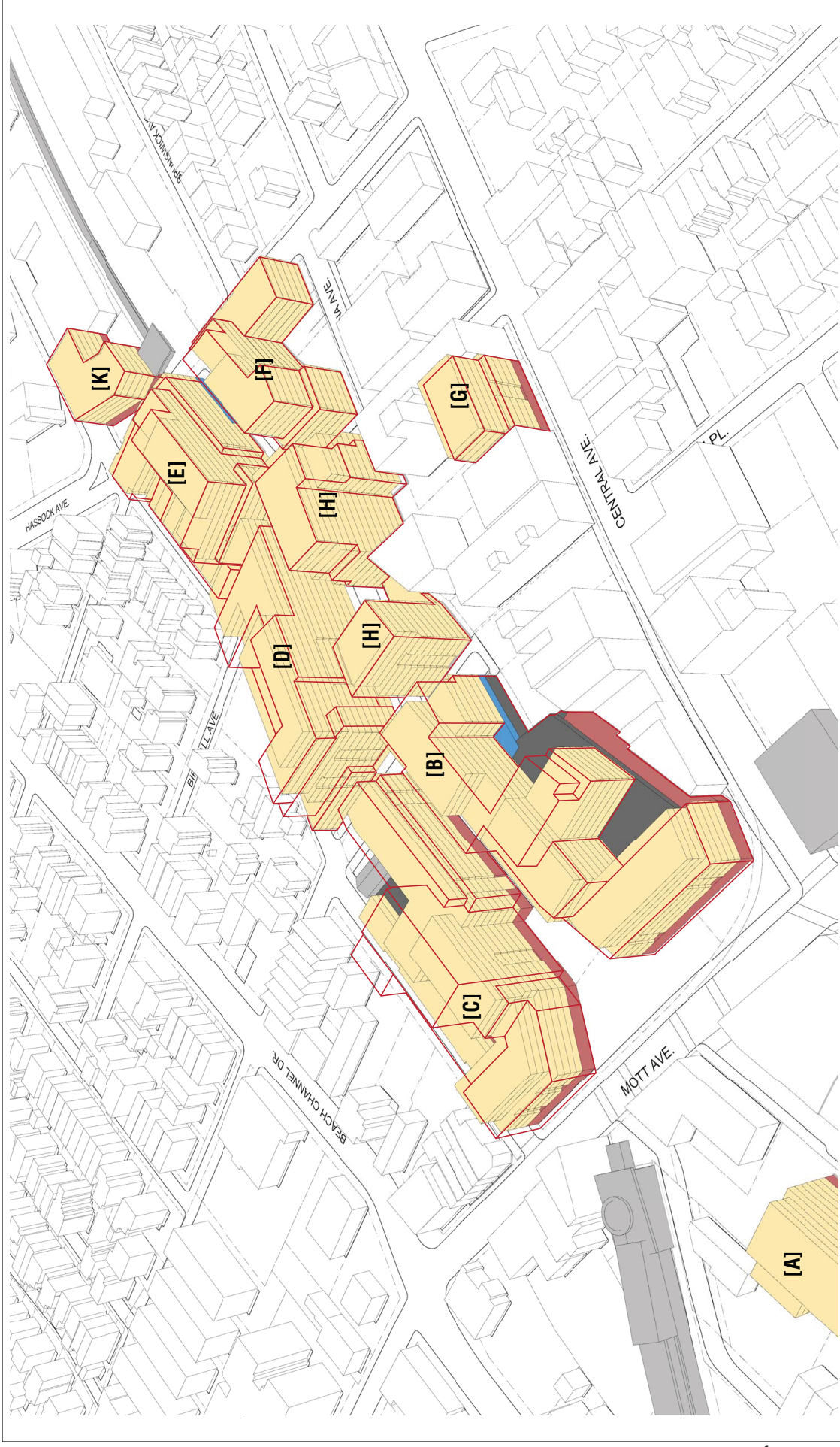


Looking North



Looking South

- Community Facility Use
- Commercial (Retail) Use
- Residential Use
- Parking



- [] Massing changes
- Community Facility Use
- Commercial (Retail) Use
- Residential Use
- Parking
- [] Building Letter

A-Application Changes to
Proposed DFRURA, View Looking North
Figure 8a



- - - Massing changes
- Community Facility Use
- Commercial (Retail) Use
- Residential Use
- Parking
- [A]** Building Letter

A-Application Changes to
Proposed DFRURA, View Looking South
Figure 8b

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- Building C: The base of the building along Mott Avenue would be reduced by one story and set back three feet from the lot line. The tallest tower would be reduced by 4 stories (from 15 to 11 stories), and the remaining building rising to 8 stories. A portion of the building façade along Redfern Avenue also would be reduced by one story.
- Building D: The towers at the center of the building would be increased by 3 stories (from 12 to 15 stories and from 9 to 12 stories). The floorplates of the towers would be reduced by increasing the depth of the setback from the façade along Redfern Avenue.
- Building E: A portion of the tower along the proposed extension of Birdsall Avenue would be reduced 3 stories (from 12 to 9 stories).
- Building H: The southernmost tower would remain 12 stories, but with a setback after 8 stories instead of rising without setbacks. Along the proposed north-south street, the base of the building would be increased from 6 to 8 stories with an additional side-yard setback after 12 stories. The north tower would remain 15 stories.
- Building G: The Central Avenue facade would increase the setback by one story. The tower height would increase one story (from 9 to 10 stories).

D. REVISED ENVIRONMENTAL ANALYSES DUE TO THE AMENDED APPLICATION

This section presents revised analyses based on the Amended RWCDs, and considers whether the A-Application would result in any new or different significant adverse environmental impacts not already identified in the DEIS. The analyses find that the A-Application would not result in any new or different significant adverse impacts not already identified in the DEIS. While the Amended RWCDs would not lead to new or different significant adverse impacts, it would worsen certain significant adverse impacts identified in the DEIS related to publicly funded child care centers, open space, traffic, transit, and construction noise.

PROBABLE IMPACTS OF THE A-APPLICATION

Based on the varied likely effects of the A-Application, the Amended With-Action Condition is discussed separately below for each CEQR category, as appropriate.

LAND USE, ZONING, AND PUBLIC POLICY

The DEIS concluded that the Proposed Actions would result in beneficial land use effects through the facilitation of mixed-use, transit-oriented development, new mixed-income housing, and publicly accessible open space. The A-Application is similarly intended to improve land use conditions by facilitating activity, the development of a mixed-use neighborhood, and reestablishing the area as the hub of the Rockaway peninsula. The proposed zoning map and text amendments that relate to land use, zoning and public policy include the proposed zoning map changes that modify the RWCDs program assumptions for Projected Development Site 9, and that result in the addition of two Projected Development Sites (Sites 18 and 19).

Land Use

Projected Development Sites 18 and 19 are located adjacent to the southeastern and northeastern boundary, respectively, of the DEIS Rezoning Area and are mapped within the A-Application Rezoning Area (see **Figure 2**). Projected Development Site 18, located on the southeast corner of Gateway Boulevard and Mott Avenue (Block 15574, Lot 36), is approximately 19,362 square

feet, currently vacant and located within an R5 zoning district (1.25 Floor Area Ratio [FAR]). Historically, the site was utilized as a playground; however, it was later fenced in and closed due to gang- and drug-related activities. The property is owned by the same entity that controls the adjacent property to the northwest of the site—Projected Development Site 9 on Block 15563, Lots 31, 40 and 43. The development proposed for Site 18 would include a 7-story, 70,814-gsf residential building with 50 two- and three-bedroom units;³ an indoor playroom and recreation/garden space for residents' use; and a publicly accessible open space.⁴ The land uses that surround Projected Development Site 18 are residential with a 6-story apartment building north of the site, single family residential to the east, and a two-story apartment complex to the south demonstrating a variety of housing typologies in the immediate vicinity. The land use in the proposed program would transform underutilized land to residential use with a multi-purpose space for community use and open space amenities consistent with and complimenting the existing land uses that surround the site.

With respect to Projected Development Site 9, which is controlled by the same entity that controls Projected Development Site 18, as compared to the community facility ground-floor use analyzed in the DEIS, the Amended RWCDs now includes a retail ground-floor use instead of a community facility use. This is due to the development plan proposed for the site. As Projected Development Site 9 is contiguous with other retail uses along Mott Avenue, this change in ground floor use serves to extend the retail corridor. Therefore, the change in use is consistent with existing land uses in the Project Area.

Projected Development Site 19 (Block 15535, Lots 11, 58, 59, 60, and 61), located between Central Avenue and Augustina Avenue and immediately south of the termination of Neilson Street, is currently occupied by a supermarket, Laundromat, and rear service yard. The Site is currently located within an R5 zoning district with a C1-2 Commercial Overlay. In the Amended With-Action Condition, Projected Development Site 19 would be redeveloped with one building including a larger ground floor supermarket, a Laundromat, and residential units above. The land uses that surround Site 19 include a House of Worship (Arverne Church of God) to the north of the property and a school (The Church of God Christian Academy) adjoining the site to the southwest. Across Central Avenue, southeast, Projected Development Site 19 faces a private residential playground and parking lot that sits in between two six-story residential buildings. The rear yard of Site 19, across Augustina Avenue, faces four attached single-family residential units. The land use contemplated for Projected Development Site 19 would include a larger supermarket use on the property and add additional residential units to the area.

Similar to the land uses analyzed in the DEIS, the amended ground floor use on Projected Development Site 9 and the uses proposed at Projected Development Sites 18 and 19 are similar to and complement the existing uses in their immediate areas.

The Primary Study Area identified in the DEIS included areas 400 feet outside of the Project Area boundary and therefore was inclusive of Projected Development Sites 18 and 19. The

³ For purposes of a more conservative analysis and consistent with the analytic framework used for the DEIS, the amended RWCDs assumes 1 unit per 1,000 gsf of residential space (equating to 71 dwelling units [DUs], greater than the 50 units contemplated by the property owner).

⁴ The developer of Site 18 intends to include an estimated 6,432-sf publicly accessible active open space as part of the Site 18 development program. However, the provision of publicly accessible open space would not be required, and therefore is not accounted for quantitatively in the Amended RWCDs.

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Secondary Study Area as identified in the DEIS extended approximately ¼-mile (approximately 1,320 feet) from the Project Area boundary. The anticipated mix of new uses in the Project Area with the Amended RWCDs would continue to be consistent with and complementary to existing uses and development trends within the Primary Study Area, which are primarily residential. Therefore, the A-Application would not result in any significant adverse land use impacts in the Primary Study Area.

Zoning

The proposed zoning map amendments would extend zoning changes that were proposed and analyzed in the DEIS: an extension of a C2-4 overlay to apply to Projected Development Site 9; an extension of the R6 district to encompass Projected Development Site 18; and for Site 19, an extension of the R6 district and C2-4 commercial overlay (see **Figure 2**). Similar to the DEIS, the A-Application intends to maintain and encourage diversified commercial uses and unlock development potential for Downtown Far Rockaway. The zoning map amendments advanced by the A-Application would maintain the concentration of density close to the downtown commercial core and mass transit, integrate development with the existing neighborhood scale and preserve Downtown Far Rockaway's "village character."

The Amended RWCDs would be compatible with the surrounding residential districts and be similar in use and scale to existing zoning districts. Similar to the DEIS, the proposed zoning amendments build upon previous contextual zoning changes in the Rockaway Park Community approved in November 1989, the Far Rockaway/Mott Creek rezoning, which was approved in September 2005, and the Bayswater neighborhood approved in April 2006, and the 2008 *Rockaway Neighborhoods Rezoning* plan which aim to preserve established neighborhood development patterns and prevent inconsistent development.

Land Use and Zoning

The A-Application would not result in significant adverse impacts on land use or zoning. The zoning changes that are introduced are compatible with the City's land use, zoning and public policy objectives for the area. The proposed ground floor commercial use on Projected Development Site 19 and retail use on Projected Development Site 9 would complement the downtown setting and provide a local service to the immediate community. The development resulting from the proposed zoning would enhance the vitality of existing commercial corridors, creating opportunities for a more vibrant, mixed-use community.

Public Policy

Similar to the Proposed Actions presented in the DEIS, development resulting from the A-Application would be compatible with the applicable public policies within the Project Area, Primary and Secondary Study Areas.

Housing New York

Under the requirements of MIH, the A-Application would bring additional affordable housing to Downtown Far Rockaway and directly support the goals of *Housing New York* by creating new housing opportunities on underutilized private sites. The Amended RWCDs includes 190 more affordable housing units than the RWCDs analyzed in the DEIS.

The Downtown Far Rockaway Urban Renewal Area Plan

Similar to the DEIS, the A-Application includes designation and approval of the Proposed DFRURA and Downtown Far Rockaway Urban Renewal Plan (DFRURP) for Downtown Far

Rockaway. The proposed text amendments would result in adjustments to the massing of development projected for the Proposed DFRURA, but the overall scale and program would not be changed. As with the Proposed Actions in the DEIS, the A-Application includes designation of the urban renewal area, which is necessary for the execution of the Proposed Project and would be consistent with the goals of revitalizing the Downtown Far Rockaway neighborhood and commercial core.

Housing New York: A Five-Borough, Ten-Year Plan

Similar to the DEIS, the A-Application directly supports the goals and principles outlined in *Housing New York* as the Proposed Actions, specifically, foster diverse livable neighborhoods; build new affordable housing for all New Yorkers.

OneNYC

Similar to the DEIS, the A-Application advances the goals of OneNYC through the focus on growth, equity, sustainability and resiliency. Goals outlined in the report include those related to housing (ensuring access to affordable, high-quality housing) and thriving neighborhoods (ensuring that neighborhoods will be well-served). These goals are consistent with the incremental uses under the Amended RWCDS.

Projected Development Sites 18 and 19 are located outside of the current and projected Federal Emergency Management Agency (FEMA)-designated preliminary 100-year floodplain, the area with a 1-percent-annual-chance of flooding (see **Figure 9**).

Waterfront Revitalization Program

The amended Project Area is within the City's Coastal Zone and therefore is subject to review for consistency with the policies of the City's Waterfront Revitalization Program (WRP). Generally, the WRP includes policies regarding economic development, environmental preservation, and public use of the waterfront and seeks to minimize the conflicts among these objectives. The WRP Consistency Assessment Form (see **Appendix C**) lists the WRP policies and indicates whether the A-Application would promote or hinder that policy.

This section provides additional information for the policies that have been checked "promote" or "hinder" in the WRP consistency assessment form.

Policy 1: Support and facilitate commercial and residential redevelopment in areas well-suited to such development.

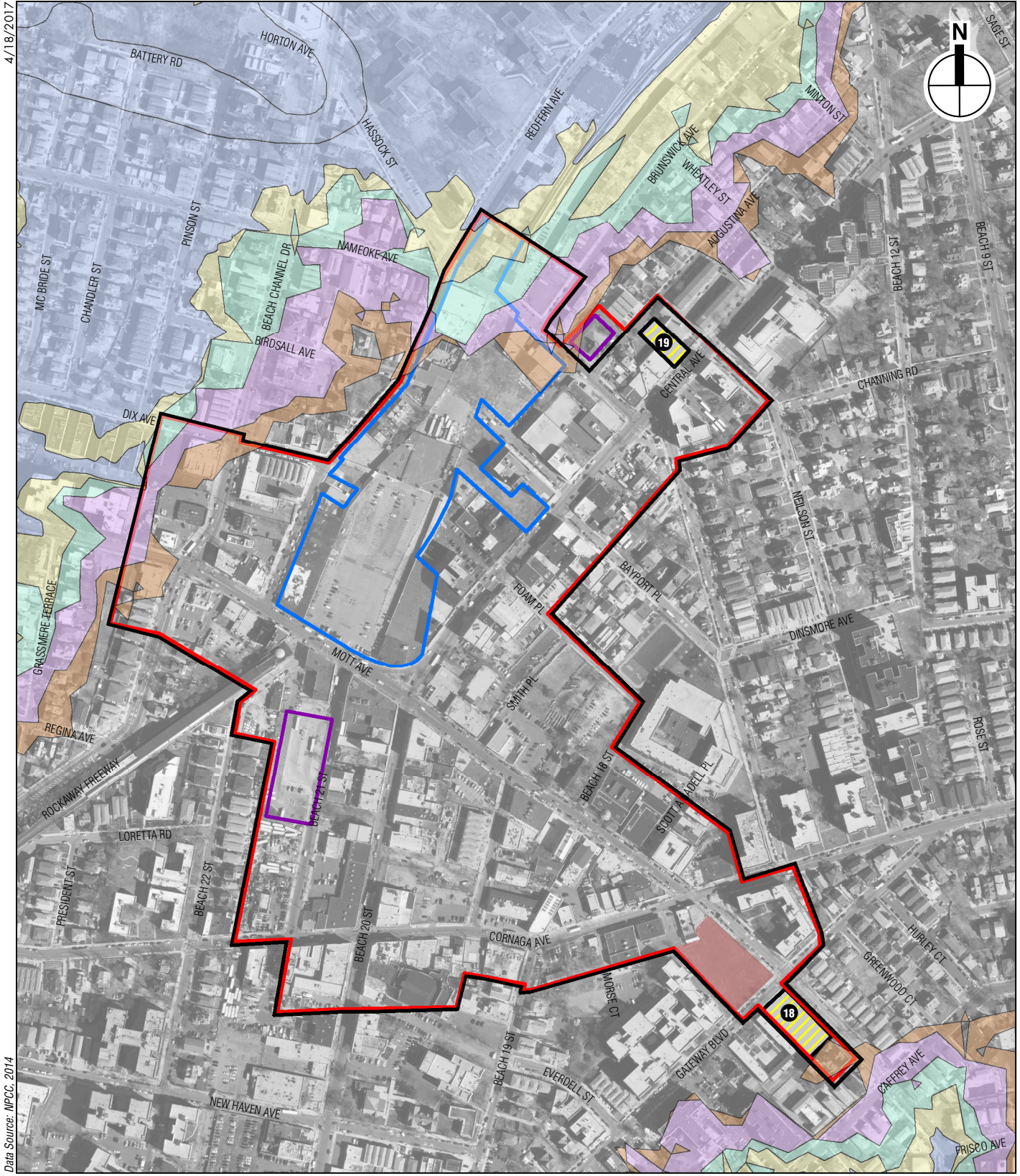
Policy 1.1: Encourage commercial and residential redevelopment in appropriate Coastal Zone areas.

Projected Development Sites 9 and 18 would be developed adjacent to properties of residential use and therefore introduce land use and zoning consistent with the surrounding area. Projected Development Site 19 would be developed in proximity to a mix of community facility, commercial, and residential uses and also introduce land use and zoning consistent with the surrounding area. Both sites are served well by public transportation. Therefore, like the Proposed Actions analyzed in the DEIS, the A-Application and Amended RWCDS are consistent with this policy.

Policy 1.3: Encourage redevelopment in the Coastal Zone where public facilities and infrastructure are adequate or will be developed.

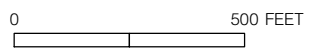
The Amended RWCDS presents a density that is compatible with the capacity of the surrounding roadways, mass transit, and some other essential community services. However, as per *CEQR Technical Manual* guidelines, the Amended RWCDS would result in

4/18/2017



Data Source: NPCC, 2014

- Project Area
- Proposed DFRURA
- A-Application Projected Development Sites
- Disposition Sites
- 1% Preliminary FIRM
- 1% Annual Probability Flood, Projected--2020s
- 1% Annual Probability Flood, Projected--2050s
- 1% Annual Probability Flood, Projected--2080s
- 1% Annual Probability Flood, Projected--2100



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Projected Potential Flood Zones by Year
Figure 9

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significant adverse impacts on child care facilities. As described in the “Community Facilities” section below, the Amended RWCDs would slightly exacerbate the significant adverse impact on child care facilities identified in the DEIS. Please refer to DEIS Chapter 21, “Mitigation” for a description of potential measures to mitigate this identified significant adverse impact.

As discussed in the DEIS, upgrades to the water and sewer infrastructure are required in the Project Area in order to provide adequate service to certain Projected Development Sites. Several sewer infrastructure capital improvement projects are planned within the Project Area; in particular, these projects will include construction of new storm sewers in areas currently lacking storm sewer service. To the extent that the planned improvements do not provide storm sewer service to sites not currently served by a storm sewer, any new development at those sites would be required to connect the sites to DEP’s storm sewers in accordance with the City’s approved drainage plan. Furthermore, DEP’s approval and sign-off would be required for each development in the Amended With-Action Condition to obtain building permits. However, as with the other developments, any new development on these sites would be required to implement stormwater best management practices (BMPs) as part of the DEP site connection approval process in order to bring the building(s) into compliance with the required stormwater release rate. These BMPs, which may include planted rooftop spaces (“green roofs”) and/or vaults, would ensure that the developments discharge at a rate that would not exacerbate the surcharged condition of the downstream storm sewers, in accordance with DEP regulations.

Based on a review of DEP records, sanitary and storm sewers extend along Mott Avenue near Projected Development Site 18. Projected Development Site 19 is a through-block site located across the street from Project Development Site 14, with frontages on Central Avenue and Augustina Avenue. As discussed in the DEIS, sanitary and storm sewers extend along Central Avenue in this portion of the Project Area. Therefore, existing sewer infrastructure is available for the two additional projected developments in the Amended RWCDs. Therefore, like the Proposed Actions analyzed in the DEIS, the A-Application is consistent with this policy.

Policy 1.5: Integrate consideration of climate change and sea level rise into the planning and design of waterfront residential and commercial development, pursuant to WRP Policy 6.2.

As shown in **Figure 9**, Projected Development Sites 18 and 19 are not within the existing 100-year floodplain (Zone AE; the area with a one percent probability of flooding each year) or the existing 500-year floodplain (Zone X; the area with a 0.2 percent probability of flooding each year). Projected Development Sites 18 and 19 are not located in the existing or projected floodplain, and therefore, analysis of resilience to potential future coastal flooding events is not required. Overall, the A-Application would not materially change the conclusions presented in the DEIS; therefore the A-Application is consistent with this policy.

Policy 5: Protect and improve water quality in the New York City coastal area.

Policy 5.1: Manage direct or indirect discharges to waterbodies.

For Development Sites 18 and 19, all effluent discharges that result from heating, air conditioning and industrial facilities would be managed properly and would not cause negative impacts to fish and wildlife habitats in Jamaica Bay or the surrounding areas or alter the findings of the Jamaica Bay Watershed Protection Plan Tracking Form (see

Appendix F of the DEIS) other than adding in the existing and proposed surface area for pervious and impervious cover types within the two sites. Therefore, the A-Application would be consistent with this policy.

As discussed in Policy 1.3, the City's drainage plan will be amended to reflect the sewer infrastructure improvements that are needed to support future development based on results of a hydraulic analysis DEP is currently completing. These infrastructure improvements would manage direct stormwater discharges to waterbodies; therefore, the A-Application is consistent with this policy.

Policy 5.2: Protect the quality of New York City's waters by managing activities that generate nonpoint source pollution.

As discussed above under Policy 1.3, DEP's approval and sign-off would be required for Projected Development Sites 18 and 19 to obtain building permits. Furthermore, Development Sites 18 and 19 would require stormwater BMPs to control the rate of stormwater discharge from the sites to DEP storm sewers as a part of the DEP site connection approval process. Therefore, the A-Application and Amended RWCDs are consistent with this policy.

Policy 5.5: Protect and improve water quality through cost-effective grey-infrastructure and in-water ecological strategies.

As with the other developments, Projected Development Sites 18 and 19 would require stormwater BMPs to control the rate of stormwater discharge from the sites to DEP storm sewers. These BMPs may include planted rooftop spaces ("green roofs") and/or vaults to control the rate of discharge to the City storm sewer. Sewer system improvements will be made in the Project Area in order to provide adequate service. Any development on a site not currently served by a storm sewer would be required to build a private sewer or drain conforming to the City's approved drainage plan. These sewer system improvements would support current residents and future growth, and would optimize the existing sewer system, therefore the A-Application is consistent with this policy.

Policy 6: Minimize loss of life, structure, infrastructure, and natural resources caused by flooding and erosion, and increase resilience to future conditions created by climate change.

Policy 6.1: Minimize losses from flooding and erosion by employing non-structural and structural management measures appropriate to the site, the use of the property to be protected, and the surrounding area.

As described above, Projected Development Sites 18 and 19 are not within the existing 100-year floodplain (Zone AE; the area with a one percent probability of flooding each year) or the existing 500-year floodplain (Zone X; the area with a 0.2 percent probability of flooding each year). Given that Projected Development Sites 18 and 19 are not located in the existing or projected floodplain, analysis of resilience to potential future coastal flooding events is not required. Overall, the Amended With Action Condition would not materially change the conclusions presented in the DEIS; and the A-Application is consistent with this policy.

Policy 6.2: Integrate consideration of the latest New York City projections of climate change and sea level rise (as published in New York City Panel on Climate Change 2015 Report, Chapter 2: Sea Level Rise and Coastal Storms) into the planning and design of project in the city's Coastal Zone.

As discussed above, Projected Development Sites 18 and 19 fall outside of the existing 100-year floodplain (Zone AE; the area with a one percent probability of flooding each year) and

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the existing 500-year floodplain (Zone X; the area with a 0.2 percent probability of flooding each year). Therefore, the A-Application and Amended RWCDs are consistent with this policy.

Policy 7: Minimize environmental degradation and negative impacts on public health from solid waste, toxic pollutants, hazardous materials, and industrial materials that may pose risks to the environment and public health and safety.

Policy 7.1: Manage solid waste material, hazardous wastes, toxic pollutants, substances hazardous to the environment, and the unenclosed storage of industrial materials to protect public health, control pollution and prevent degradation of coastal ecosystems.

To reduce the potential for adverse impacts associated with new construction in the Amended With-Action Condition, further environmental investigations would be required at sites where a high or moderate potential for contamination were identified (i.e., suspected fill materials containing ash, and nearby petroleum storage, auto repair and/or dry cleaning). To ensure that these investigations are undertaken, hazardous materials (E) designations would be placed on Projected Development Sites 18 and 19. These (E) designations require the owners of the properties to do the following prior to obtaining NYC Department of Buildings (DOB) permits for new development entailing soil disturbance or for changes to a more sensitive building use (e.g., from non-residential to residential):

- Conduct a Phase I ESA in accordance with the American Society of Testing Materials (ASTM) E1527-13, where one was not previously conducted or where required by the Mayor's Office of Environmental remediation (OER) based on the date of the previous assessment;
- Prepare and implement a soil and groundwater testing protocol approved by OER;
- Where appropriate, conduct remediation in accordance with an OER-approved Remedial Action Plan (RAP) and Construction Health and Safety Plan (CHASP) to the satisfaction of the OER; and
- Prepare a post-construction Remedial Closure Report (RCR) documenting compliance with the RAP/CHASP, to obtain a Notice of Satisfaction and Certificates of Occupancy for newly constructed structures.

With the implementation of the measures required by the (E) designations, Projected Development Sites 18 and 19 would not result in any significant adverse impacts with respect to hazardous materials; therefore the Amended RWCDs is consistent with this policy.

Policy 7.2: Prevent and remediate discharge of petroleum products.

Potential petroleum storage tanks were identified in proximity to Projected Development Sites 18 and 19. Legal requirements, such as those relating to petroleum storage tank maintenance and handling and disposal of asbestos-containing materials (ACM), lead-based paint (LBP) and polychlorinated biphenyl (PCBs), would continue to be applicable. The A-Application would require that the removal of any encountered tanks be performed in accordance with applicable regulatory requirements including New York State Department of Environmental Conservation (NYSDEC) requirements relating to spill reporting and tank registration. Therefore, the A-Application and Amended RWCDs is consistent with this policy.

Policy 7.3: Transport solid waste and hazardous materials and site solid and hazardous waste facilities in a manner that minimizes potential degradation of coastal resources.

Similar to development associated with the RWCDS analyzed in the DEIS, potential hazardous materials generated at Projected Development Sites 18 and 19 would be remediated and disposed of in conformance with all applicable laws, rules, and regulations, thus avoiding the potential for adverse impacts on the coastal zone resources; as such, the A-Application is consistent with this policy.

Policy 10: Protect, preserve, and enhance resources significant to the historical, archaeological, architectural, and cultural legacy of the New York City coastal area.

Policy 10.1: Retain and preserve historic resources, and enhance resources significant to the coastal cultural of New York City.

There are no known architectural resources located on Projected Development Sites 18 and 19. Therefore, the A-Application is consistent with this policy.

Policy 10.2: Protect and preserve archeological resources and artifacts.

A letter from the NYC Landmarks Preservation Commission (LPC) dated March 29th, 2017, found that Projected Development Sites 18 and 19 have no archaeological significance and no further assessment is warranted; therefore the A-Application is consistent with this policy.

SOCIOECONOMIC CONDITIONS

As described in the DEIS Chapter 3 “Socioeconomic Conditions,” the Proposed Actions would not result in significant adverse impacts with respect to any of the five areas of socioeconomic concern: direct residential displacement; indirect residential displacement; direct businesses displacement; indirect businesses displacement; and adverse effects on specific industries. Similarly, as discussed below, the Amended RWCDS would not result in any significant adverse socioeconomic impacts.

The proposed modifications to the Special District text would result in adjustments in massing on the Proposed DFRURA, but would not affect the RWCDS program for the DFRURA. As such, the proposed text amendments would have no effect on any of the five socioeconomic areas of concerns. The analysis below focuses on the programing changes associated with the Amended RWCDS; specifically, the proposed zoning map changes, which result in a modification to the ground floor use on Projected Development Site 9, and new additional development on Projected Development Sites 18 and 19.

Direct Residential Displacement

There are currently no residential uses located on Projected Development Sites 18 and 19. Therefore, the Amended RWCDS would result in the same amounts of potential direct residential displacement as analyzed in the DEIS. Under the RWCDS analyzed in the DEIS, by 2032 the Proposed Actions could directly displace an estimated 17 residents living in eight DUs. According to the *CEQR Technical Manual*, direct displacement of less than 500 residents would not typically be expected to alter the socioeconomic character of a neighborhood. The 17 potentially displaced residents represent less than one-half of one percent of the Socioeconomic Study Area population, and their displacement would not have the potential to alter the socioeconomic character in the Study Area. Therefore, like the Proposed Actions analyzed in the DEIS, the Actions proposed in the A-Application would not result in significant adverse impacts due to direct residential displacement.

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Direct Business Displacement

While the Amended RWCDS could result in additional direct business displacement as compared to the RWCDS analyzed in the DEIS, the potential displacement would not result in significant adverse socioeconomic impacts. Projected Development Site 18 does not contain any businesses, and therefore no direct business displacement would result from the development of Site 18. Projected Development Site 19 is currently improved with an approximately 1,000-gsf laundromat and an approximately 3,000-gsf supermarket, both of which would be temporarily displaced during construction, with the businesses replaced by a larger supermarket and a laundromat of comparable size upon redevelopment of Site 19. Based on standard employment ratios used in the DEIS, the two existing businesses on Projected Development Site 19 employ an estimated 12 workers. In the aggregate, the Amended RWCDS could result in the direct displacement of an estimated 295 employees in 31 businesses.

As described in the DEIS, while all potentially displaced businesses create value to the local economy, their products and services are not essential to the local economy as defined by CEQR and would continue to be available through other study area businesses. With respect to two additional businesses potentially displaced with the Amended RWCDS, there are several laundromats within close proximity to the existing Projected Development Site 19 location, including Elon Cleaners on Cornaga and Mott Avenues; Klean and Kleaner Laundromat on Beach Channel Drive near Mott Avenue; and Clean Rite Center on Beach Channel Drive near Hassock Street. With the Amended RWCDS, the program for Site 19 includes a laundromat of comparable size. With respect to the displacement of a grocery store use (in this case, the Cruz Food Corporation), there are several comparable grocery stores within the socioeconomic study area including 10 small convenience grocers as well as large-format stores such as Key Food, Bravo Supermarket and C-Town Supermarket. In addition, Projected Development Site 19 would be redeveloped with a grocery store projected to be more than twice the size of the supermarket that would be displaced.

Indirect Residential Displacement

Similar to the Proposed Actions analyzed in the DEIS, the A-Application would result in additional residential development that could influence market conditions. However, with 71 affordable DUs proposed on Projected Development Site 18, 27 affordable DUs proposed on Projected Development Site 19, and 133 affordable DUs proposed on Projected Development Site 9 (as compared to 135 DUs including 41 affordable DUs as analyzed in the DEIS), the population introduced by the Amended RWCDS would have an overall average income lower than the population introduced by the Proposed Actions as analyzed in the DEIS, and closer to that of the existing residential population. In this respect, the Amended RWCDS would have less potential to alter the demographic composition of the Socioeconomic Study Area as compared to the RWCDS analyzed in the DEIS. Furthermore, additional affordable DUs would ensure a mix of incomes within the neighborhood and could offset rent pressures introduced by the market-rate housing. Therefore, the Actions proposed in the A-Application would not result in significant adverse impacts due to indirect residential displacement.

Indirect Business Displacement

Similar to the RWCDS analyzed in the DEIS, the Amended RWCDS would introduce a substantial new residential population that could influence commercial rents. However, as described above, the residential population in the Amended With-Action Condition would have incomes that more closely reflect incomes of the existing Socioeconomic Study Area population.

The additional affordable housing introduced by the A-Application could therefore serve to maintain and preserve a mix of retail use types and price points in the Study area. Overall, the incremental commercial and residential uses would not be expected to materially change the DEIS finding that the Proposed Actions would not result in significant adverse impacts due to indirect business displacement.

Adverse effects on specific industries

The A-Application would not significantly affect the business conditions in any specific industry or any category of business. The Amended RWCDS would result in the direct displacement of two businesses and an estimated 12 employees from Projected Development Site 19; throughout the Project Area, the Amended RWCDS could directly displace an estimated 31 businesses employing approximately 295 workers. The potentially displaced businesses do not represent a critical mass of businesses within any City industry or category of business. Although these businesses are valuable individually and collectively to the City's economy, the goods and services offered by potentially displaced businesses can be found elsewhere within the Socioeconomic Study Area, within a broader trade area, and/or within the City as a whole. Furthermore, the products and services offered by the businesses that would be directly displaced are not expected to be essential to the viability of other businesses within or outside the Study Area. Finally, the Amended RWCDS, through direct or potential indirect business displacement, would not substantially reduce employment or have an impact on the economic viability in any City industry or category of business.

COMMUNITY FACILITIES AND SERVICES

The RWCDS analyzed in the DEIS did not exceed the thresholds requiring analyses of health care facilities or fire and police protection services, indicating that there would be no significant adverse impacts on these facilities. Similarly, the Amended RWCDS associated with the A-Application does not exceed thresholds requiring analyses of health care facilities or police and fire protection services, indicating that there would be no significant adverse impacts on these facilities.

Like the RWCDS analyzed in the DEIS, the Amended RWCDS exceeds the thresholds for analysis of elementary, intermediate and high schools; libraries; and child care facilities. This section analyzes the Amended RWCDS to determine whether the A-Application would result in any new or different community facility impacts not identified in the DEIS.

Public Schools

The DEIS analysis of public schools found that the Proposed Actions would not result in significant adverse impacts to school capacity at the elementary, intermediate, or high school levels. The A-Application could result in more school-aged children—the Amended RWCDS assumes an incremental increase of 3,123 DUs which is 96 DUs greater than the RWCDS analyzed in the DEIS. Therefore, an analysis of the Amended RWCDS is warranted.

Based on enrollment rates contained in the *CEQR Technical Manual*, the Amended RWCDS would introduce approximately 874 elementary students and 375 intermediate school students to Sub-district 1 of Community School District (CSD) 27 (illustrated in DEIS Figure 4-1). The amended RWCDS would also introduce approximately 437 high school students to the borough of Queens.

As shown in **Table 6**, in the Amended With-Action Condition the total elementary school enrollment of Sub-district 1/CSD 27 would increase to 5,336 (95.0 percent utilization) with a

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surplus of 286 seats. The total intermediate school enrollment of Sub-district 1/CSD 27 would increase to 2,490 (92.9 percent utilization) with a surplus of 190 seats. The total high school enrollment for the borough of Queens would increase to 74,377 (107.5 percent utilization) with a deficit of 5,183 seats.

Table 6
Estimated Public School Enrollment, Capacity, and Utilization:
Amended With-Action Condition

Study Area	No Action Enrollment	Students Introduced by the Amended RWCDs	Total Amended With-Action Enrollment	Capacity	Available Seats	Utilization	Change in Utilization Compared with No Action
Elementary Schools							
Sub-district 1 of CSD 27	4,462	874	5,336	5,622	286	95.0%	15.6%
Intermediate Schools							
Sub-district 1 of CSD 27	2,115	375	2,490	2,680	190	92.9%	14.0%
High Schools							
Queens Borough	73,940	437	74,377	69,194	-5,183	107.5%	0.6%
Sources: DOE <i>Enrollment Projections 2015-2024 by the Grier Partnership</i> ; DOE, <i>Utilization Profiles: Enrollment/Capacity/Utilization, 2015-2016</i> , DOE <i>2015-2019 Proposed Five-Year Capital Plan</i> , Amended November 2016; School Construction Authority (SCA); NYC SCA Projected New Housing Starts as used in 2015-2024 Enrollment Projections.							

Based on *CEQR Technical Manual* guidelines, a significant adverse impact would be identified if the Amended RWCDs resulted in both of the following conditions: (1) a utilization rate of the elementary or intermediate schools in the sub-district that is equal to or greater than 100 percent in the Amended With-Action Condition; and (2) an increase of five percentage points or more in the collective utilization rate between the No Action and the Amended With-Action Conditions. For high schools, a significant adverse impact would be identified if the Amended RWCDs would result in both of the following conditions: (1) a utilization rate of the high schools in the borough of Queens that is equal to or greater than 100 percent in the Amended With-Action Condition; and (2) an increase of five percentage points or more in the collective utilization rate between the No Action and the Amended With-Action Conditions.

In the Amended With-Action Condition, elementary and intermediate schools would continue to operate with a surplus of seats (see **Table 6**). The increase in utilization attributable to the Amended RWCDs would be approximately 15.6 percentage points for elementary schools and 14.0 percentage points for intermediate schools. Although these increases in utilization would be above the five percentage-point-change threshold, the overall utilization rate would be under 100 percent for both elementary and intermediate schools. High schools within the borough of Queens would continue to operate with a deficit of seats. However, the increase in utilization attributable to the Amended RWCDs would be approximately 0.6 percent, which is below the five percentage-point-change threshold outlined by the *CEQR Technical Manual*. Therefore, the Actions proposed in the A-Application would not result in a significant adverse impact on public schools.

Libraries

The DEIS analysis found that the Proposed Actions would not result in significant adverse impacts to libraries. The A-Application would result in an increase number of residents demanding library services—the Amended RWCDs assumes an incremental increase of 3,123 DUs which is 96 DUs greater than the RWCDs analyzed in the DEIS. Therefore, an analysis of the Amended RWCDs is warranted.

Based on an average household size of 2.71 for Queens Community District 14, by 2032 the A-Application would result in an incremental increase of approximately 8,463 new residents. **Table 7** provides the population increase and the change in the holding-per-resident ratio for the library catchment area (illustrated in DEIS Figure 4-2). With this additional population, the Far Rockaway Library would serve 67,054 residents (approximately a 14.4 percent increase). Under the Amended RWCDs, the holdings per resident ratio for the Far Rockaway Library catchment area would decrease from approximately 1.73 to 1.51.

Table 7
Amended With-Action Condition: Catchment Area Population

Library Name	Catchment Area Population – Future Without the Proposed Project	Population Increase due to the Amended Proposed Project	Catchment Area Population with the Amended Proposed Project	Population Increase	Holdings per Resident
Far Rockaway Library	58,591	8,463	67,054	14.4%	1.51
Sources: NYPL (2013); U.S. Census Bureau, 2010 Census; 2011-2015 American Community Survey Five-year Estimates; AKRF, Inc.					

For the Far Rockaway Library, the catchment area population increases attributable to the Amended RWCDs is above the five percent threshold cited in the *CEQR Technical Manual*. Therefore, the A-Application could result in a noticeable change in the delivery of library services. However, although the Far Rockaway Library catchment area population would increase by nearly 15 percent, this increase would not be expected to impair the delivery of library services due to access to nearby libraries, the QBPL inter-library loan system and the anticipated upgrades at the Far Rockaway Library. Residents would have access to the entire Queens Library through the inter-library loan system and could have volumes delivered directly to their nearest library branch. Residents would also have the option of utilizing other nearby library branches such as the Arverne Queens Library Branch approximately two miles away, and the Queens Teen Library, one block south of the Far Rockaway Library. In addition, the renovation of the Far Rockaway Library is anticipated to be complete by 2019. The renovations are expected to upgrade all library services, double the amount of library space and increase its capacity to serve a larger residential catchment area. Therefore, similar to the Proposed Actions analyzed in the DEIS, the Actions proposed in the A-Application would not have a significant adverse impact on libraries.

Child Care

The DEIS analysis estimated that the RWCDs would generate 194 additional children under the age of six who would be eligible for publicly-funded child care programs. With the addition of these children, child care facilities in the study area (illustrated in DEIS Figure 4-3) would operate at 146.9 percent utilization, with a deficit of 181 slots, resulting in a significant adverse impact on child care facilities. The Amended RWCDs would increase the total number of affordable DUs by 190, resulting in 27 additional eligible children to the Child Care study area.⁵

⁵ Consistent with DEIS methodology, it is conservatively assumed that all affordable units generated by the A-Application would be eligible for publicly-funded child care services. The A-Application would introduce 98 new affordable units at Projected Development Sites 18 and 19, and with the A-Application Projected Development Site 9 is assumed to be 100 percent affordable, resulting in a net increase of 92 affordable DUs at Site 9. The 0.14 children-per-unit multiplier for the 190 incremental affordable units is based on Table 6-1b of the *CEQR Technical Manual*.

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Therefore, additional analysis is warranted to understand the extent to which the Amended RWCDs would exacerbate the significant adverse impact identified in the DEIS.

In the aggregate, the Amended RWCDs assumes that 1,580 affordable units eligible for publicly-funded child care services would be introduced to the Project Area by 2032. Based on *CEQR Technical Manual* child care multipliers, the Amended RWCDs would result in a total of approximately 222 children under the age of six who would be eligible for publicly funded child care programs.

With the addition of these children, child care facilities in the study area would operate at 154.2 percent utilization with a deficit of 209 slots (see **Table 8**). Total enrollment in the study area would increase to 595 children, compared to a capacity of 386 slots, which represents an increase in the utilization rate of 57.5 percentage points over the No Action Condition.

Table 8
DEIS RWCDs vs. Amended RWCDs:
Estimated Public Child Care Facility Enrollment, Capacity, and Utilization

	Enrollment	Capacity	Available Slots	Utilization Rate	Percentage Point Change in Utilization
No Action Condition	373	386	13	96.6%	N/A
DEIS With-Action Condition	567	386	-181	146.9%	50.3%
Amended With-Action Condition	595	386	-209	154.2%	57.5%

Source: New York City Administration for Children’s Services (ACS) June 2016.

With the Amended RWCDs child care facilities in the study area would operate over capacity by 209 children and exhibit a 57.5 percentage point increase in the utilization rate, well over the five percent threshold. Similar to the Proposed Actions analyzed in the DEIS, the A-Application would result in a significant adverse impact on child care facilities, and would grow the estimated deficit by approximately 15.5 percent (28 slots) as compared to the deficit estimated for the RWCDs in the DEIS.

As described in the DEIS, several factors may reduce the number of children in need of publicly funded child care slots in NYC Administration for Children’s Services (ACS) contracted child care facilities. Families in the study area could make use of alternatives to publicly funded child care facilities. There are slots at homes licensed to provide family-based child care that families of eligible children could elect to use instead of public center child care. As noted above, these facilities provide additional slots in the study area but are not included in the quantitative analysis. Parents of eligible children are also not restricted to enrolling their children in child care facilities in a specific geographical area and could use public child care centers outside of the study area.

Mitigation for a significant child care impact may include provision of additional suitable location(s) for a child care center within a reasonable distance, funding, or making program improvements to support additional day care facility capacity. Measures to mitigate the identified significant adverse impact on publicly funded child care centers will continue to be explored before issuance of the FEIS in coordination with the lead agency, DCP, and ACS. However, as noted in the DEIS, the potential exists that sufficient measures may not be available to fully mitigate the identified adverse impact. If, after exploring all possible mitigation measures, it is determined that the significant adverse impact on publicly funded child care facilities would not be completely mitigated, an unavoidable significant adverse impact would result.

OPEN SPACE

The DEIS found that the Proposed Actions would result in direct impacts on several open space resources due to shadows generated by RWCDs buildings, and would result in significant adverse indirect impacts to open space conditions in the ½-mile residential study area due to the RWCDs residents’ demands on open space resources. The Amended RWCDs would introduce more residents and workers to the study area as compared to the RWCDs analyzed in the DEIS, and would reduce by 5,669 sf the amount of passive open space offered by the public plaza on the Proposed DFRURA.⁶ With respect to Projected Development Site 18, it is the developer’s intent to include an estimated 6,432-square-foot publicly accessible active open space as part of the Site 18 development program. However, the provision of publicly accessible open space would not be required, and therefore is not accounted for in the Amended RWCDs for this analysis. These modifications could have indirect effects on open spaces, warranting analysis in this Technical Memorandum.

Quantitative Assessment

As shown in **Table 5** above, the Amended RWCDs includes 96 additional DUs as compared to the RWCDs analyzed in the DEIS; a 11,660-gsf increase in the retail increment; a 6,000-gsf decrease in the community facility increment; and a 5,669-sf decrease in the open space increment (from a reduction of plaza space on the Proposed DFRURA). With the Amended RWCDs, the residential population would be increased by approximately 260 residents, while the non-residential population would be increased by approximately 654 workers; as a result, the combined open space user population would increase slightly, compared to that under the DEIS RWCDs.

As shown in **Table 9**, in the Amended With-Action Condition, the non-residential (i.e., worker) passive open space ratio would be 5.22 acres per 1,000 non-residents, representing a decrease as compared to the 5.72 acres-per-1,000-non-resident ratio for the DEIS With-Action Condition; however, similar to the finding in the DEIS, the non-residential passive open space ratio for the Amended With-Action Condition is well above the City’s recommended guideline of 0.15 acres per 1,000 non-residents, and therefore is not considered a significant adverse impact.

Table 9
Amended With-Action Condition: Adequacy of Open Space Resources

Total Population	Open Space Acreage			Open Space Ratios (Acres per 1,000 People)			City Open Space Guidelines			
	Total	Active	Passive	Total	Active	Passive	Total	Active	Passive	
Non-residential (1/4-Mile) Study Area										
Workers	7,780	60.59	19.98	40.61	N/A	N/A	5.22	N/A	N/A	0.15
Residential (1/2-Mile) Study Area										
Residents	55,054	63.04	22.04	41.00	1.15	0.40	0.75	2.5	2	0.5

With the Amended RWCDs the residential total open space ratio would be 1.15 acres per 1,000 residents in the Amended With-Action Condition (see **Table 9**), which is the same as the

⁶ Not accounted for in the Amended RWCDs and open space analysis is an estimated 6,432-sf publicly accessible active open space that is intended as part of the development program for Projected Development Site 18. While it is the developer’s intent to provide this space, as it is not required, it therefore is conservatively excluded from the Amended RWCDs.

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residential total open space ratio for the DEIS RWCDs. The resulting residential active open space ratio in the Amended With-Action condition would be 0.40 acres per 1,000 residents, which would also be the same as the residential active open space ratio under the DEIS RWCDs. With the Amended RWCDs the residential passive open space ratio would be 0.75 acres per 1,000 residents, which is the same as the residential passive open space ratio for the DEIS RWCDs.

In the Amended With-Action Condition, the residential study area total and active open space ratios are both below City guidelines, and exhibit a significant percent decrease in open space ratios (by approximately 14.2 and 14.9 percent, respectively, as shown in **Table 10**). Although the ratio of passive open space to residents would be above the City guideline of 0.5 acres, the ratio would decrease by 13.8 percent in the Amended With-Action condition, well above the threshold of 5 percent. Therefore, similar to RWCDs analyzed in the DEIS, the Amended RWCDs would result in significant adverse impacts to total, active and passive open space ratios in the ½-mile residential study area.

Table 10
2032 Amended With-Action Condition: Open Space Ratios Summary

Ratio	City Guideline	Open Space Ratios (acres per 1,000 people)			Percent Change No Action to With Action Condition
		Existing Conditions	No Action Condition	With Action Condition	
Non-Residential (¼-Mile) Study Area					
Passive/Workers	0.15	6.16	6.06	5.22	-13.9%
Residential (½-Mile) Study Area					
Total/Residents	2.5	1.34	1.34	1.15	-14.2%
Active/Residents	2.0	0.48	0.47	0.40	-14.9%
Passive/Residents	0.5	0.87	0.87	0.75	-13.8%
Note: Active and Passive open space ratios for the ½-mile study area may not add to total open space ratio due to rounding.					

Qualitative Assessment

The analysis of the age distribution within the study area indicates that there may be greater burden on active open space in the residential study area because of the larger proportion of children and teenagers compared to Queens and New York City overall. The public plaza on the Proposed DFRURA, though smaller than assumed in the DEIS RWCDs, would still provide a much-needed passive amenity at the core of the downtown.

Despite these qualitative benefits, the Amended With-Action Condition—like the Proposed Actions analyzed in the DEIS—would exacerbate existing deficiencies in open space in the area and would exceed the capacity of open spaces to serve the population. Measures being considered to mitigate significant adverse open space impacts include: expanding existing parks; creating new open space on publicly-owned sites; pursuing opportunities to encourage owners of large privately-owned sites to create new open space as part of their redevelopment; making additional playgrounds accessible to the community after school hours through the Schoolyards to Playgrounds program; and/or improving existing parks to allow for more diverse programming and enhanced usability. These potential mitigation measures are currently being explored in coordination with the lead agency, DCP, and the NYC Department of Parks and Recreation (DPR) and will continue to be refined between the DEIS and FEIS.

SHADOWS

The DEIS concluded that the Proposed Actions would not result in significant adverse shadow impacts to sunlight-sensitive resources. The RWCDS analyzed in the DEIS would create incremental shadows on four existing sunlight-sensitive resources—Beach 20th Street Plaza, MS 53 Community Playground, Trinity Chapel’s original stained glass window, and Redfern Houses Playground—as well as three future sunlight-sensitive resources to be developed in the No Action condition as part of the Downtown Far Rockaway Urban Design and Streetscape Reconstruction Project—the Pedestrian Gateway, the Mott Avenue Greenstreet, and the DOT Plaza. The DEIS analysis found that although three resources—Beach 20th Street Plaza, the future Pedestrian Gateway, and the future DOT Plaza—would receive fairly substantial new shadow in certain seasons, the new shadows would not significantly alter the usability of the resources nor significantly threaten the health of their vegetation.

As illustrated in **Figures 7 and 8**, the A-Application would alter the massing of Proposed DFRURA buildings B, C, D, E, F, G and H. The A-Application would also extend the proposed Rezoning Area boundary, modifying the RWCDS program to include two additional Projected Development Sites—Sites 18 and 19. The additional Projected Development Sites 18 and 19 would both have a maximum building envelope of 95 feet above grade. Because the Proposed DFRURA buildings with shifted bulk and the additional Projected Development Sites would all reach at least 50 feet in height, a shadow assessment is necessary to determine if the Amended RWCDS buildings could cast incremental shadow on any sunlight-sensitive resources.

The shadow assessment for the Amended RWCDS (detailed below) finds that the altered massing of Proposed Building B would cast incremental shadow on the planned DOT Plaza, in addition to those shadows disclosed in the DEIS. However, the additional incremental shadows would be small in extent and last only 15 and 20 minutes on the May 6 and June 21 analysis days, respectively. The additional shadows would not change the conclusion presented in the DEIS that the DOT plaza would not experience a significant shadow impact due to development resulting from the Proposed Actions. No other modifications to Proposed DFRURA Buildings C, D, E, F and G, nor the addition of Projected Development Sites 18 and 19, would cast sunlight-sensitive resources in incremental shadow beyond those disclosed in the DEIS. Therefore the modifications to the RWCDS under the A-Application would not change the conclusion presented in the DEIS shadows analysis, and would not result in a significant shadow impact on any sunlight-sensitive resource.

Proposed DFRURA Building B

Under the Amended RWCDS, the maximum height of Proposed DFRURA Building B would decrease to 120 feet above grade, while the southernmost elevation of Building B along Mott Avenue would increase to a height of 90 feet above grade. The shifted bulk could alter the shadow path disclosed in the DEIS Tier 3 Shadow Assessment of Building B (see DEIS Figure 6-4B), which found that, without intervening buildings, Building B could potentially cast new shadow on the DOT Plaza and the Mott Avenue Greenstreet. A new Tier 3 Shadow Assessment of the Amended RWCDS found that, in the absence of intervening buildings, Proposed DFRURA Building B could cast shadow on the DOT Plaza from 6:27 AM to 6:50 AM on May 6 and from 5:57 AM to 6:50 AM on June 21. The Mott Avenue Greenstreet could be cast in new shadow originating from Building B from 6:27 AM to 8:15 AM and 2:15 PM to 5:18 PM on May 6 and from 5:57 to 9:00 AM and 1:45 PM to 6:01 PM on June 21. To assess the extent and duration of incremental shadow in addition to those disclosed in the DEIS, a detailed shadow analysis was performed for Building B.

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The detailed shadow assessment for the Amended RWCDs found that the Proposed DFRURA Building B would increase incremental shadow on DOT Plaza as compared to the RWCDs analyzed in the DEIS, but not on the Mott Avenue Greenstreet. As illustrated in **Figures 10 and 11**, additional incremental shadow originating from Building B would fall on DOT plaza from 6:30 AM to 6:50 AM on May 6 and from 6:25 AM to 6:45 AM on June 21. The additional incremental shadow would increase the total duration of incremental shadow on the DOT Plaza presented in Table 6-4 of the DEIS by 15 minutes on the May 6 analysis day and by 20 minutes on the June 21 analysis day. The total duration of incremental shadow on the DOT Plaza would increase to 9 hours and 26 minutes and 9 hours and 49 minutes, respectively. The additional new shadow from Building B would be restricted to the northernmost portion of the DOT Plaza east of Beach 22nd at Mott Avenue, an area which received less than one hour of incremental shadow on both May 6 and June 21 under the RWCDs analyzed in the DEIS. The detailed analysis performed in the DEIS also found that the vegetation within this section of the future DOT plaza would receive, at least, 5 hours of direct sunlight on the May 6 and June 21 analysis days.

The additional 15 and 20 minutes of new shadow on May 6 and June 21 originating from the Proposed DFRURA Building B under the Amended RWCDs would not prevent any additional vegetation from receiving at least 4 hours of direct sunlight throughout the day, the duration necessary to support plant life according to the *CEQR Technical Manual*. The timeframe of the additional incremental shadow, in the early morning when utilization of the open space would be low, would not alter the public's use of the open space. Therefore, when compared to the RWCDs analyzed in the DEIS, the increased incremental shadows originating from Building B under the Amended RWCDs would not change the conclusion disclosed in the DEIS. The DOT Plaza and Mott Avenue Greenstreet would not experience a significant shadow impact under the Amended RWCDs.

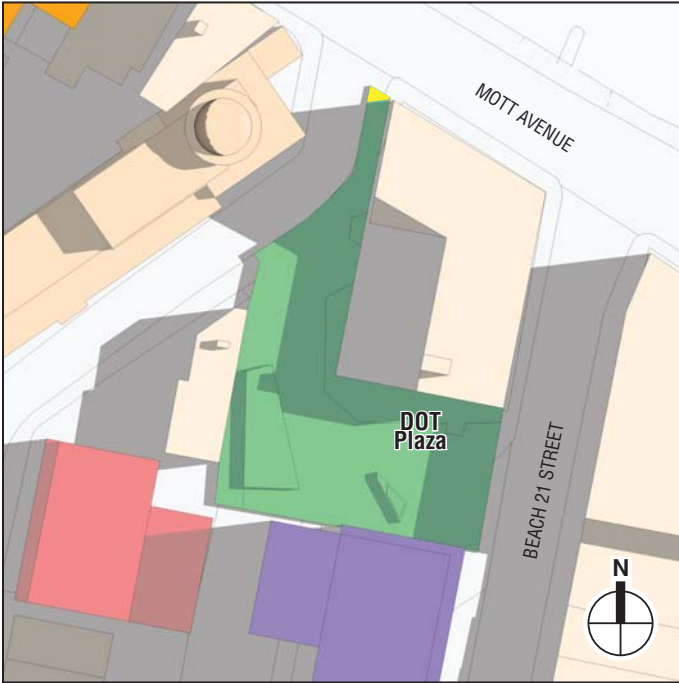
Proposed DFRURA Building C

Under the Amended RWCDs, the maximum height of Proposed DFRURA Building C would decrease to 120 feet above grade while the southernmost elevation of Building C along Mott Avenue would increase to a height of 90 feet above grade. The shifted bulk could alter the shadow path disclosed in the DEIS Tier 3 Shadow Assessment of Building C (see DEIS Figure 6-4C), which found that, without intervening buildings, Building C could potentially cast new shadow on the Pedestrian Gateway open space resource. A Tier 3 Shadow Assessment of Building C under the Amended RWCDs found that, in the absence of intervening buildings, Proposed DFRURA Building C could cast shadow on the Pedestrian Gateway only at the beginning on the March 21 analysis day from 7:36 AM to 8:00 AM. However, the detailed shadow analysis performed in the DEIS found that the all of the Pedestrian Gateway would be cast in shadow on March 21 from 7:36 AM to 8:10 AM. As a result, the shifted bulk of the Proposed DFRURA Building C could not result in any incremental shadow on the Pedestrian Gateway beyond in addition to the shadow disclosed in the DEIS. The conclusion presented in the DEIS, that the Pedestrian Gateway would not experience a significant shadows impact, would not be changed by the modifications to Proposed DFRURA Building C.

Proposed DFRURA Building D

Under the Amended RWCDs, the maximum height of Proposed DFRURA Building D would increase to 150 feet above grade, subsequently increasing the longest shadow study area disclosed in the DEIS (see DEIS Figure 6-2D). Although the longest shadow study area would

4.26.17



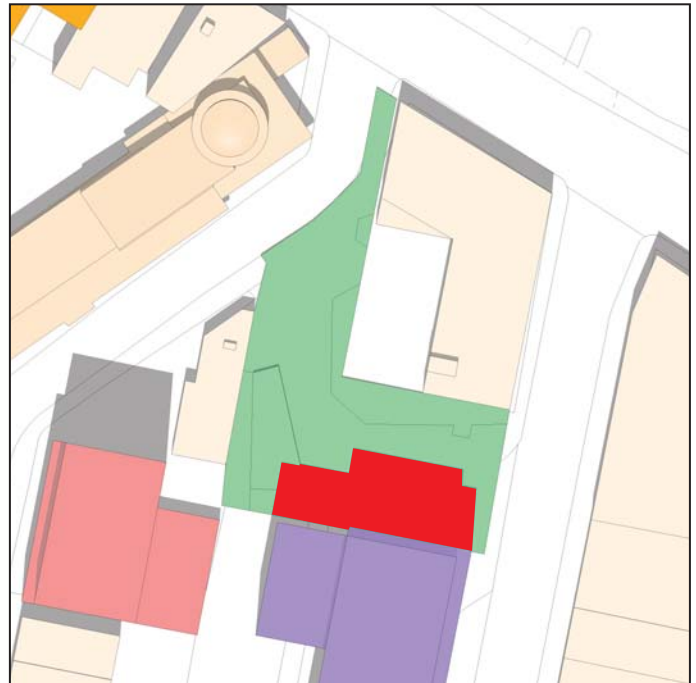
6:30 AM



8:30 AM



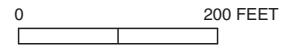
10:00 AM



12:00 PM

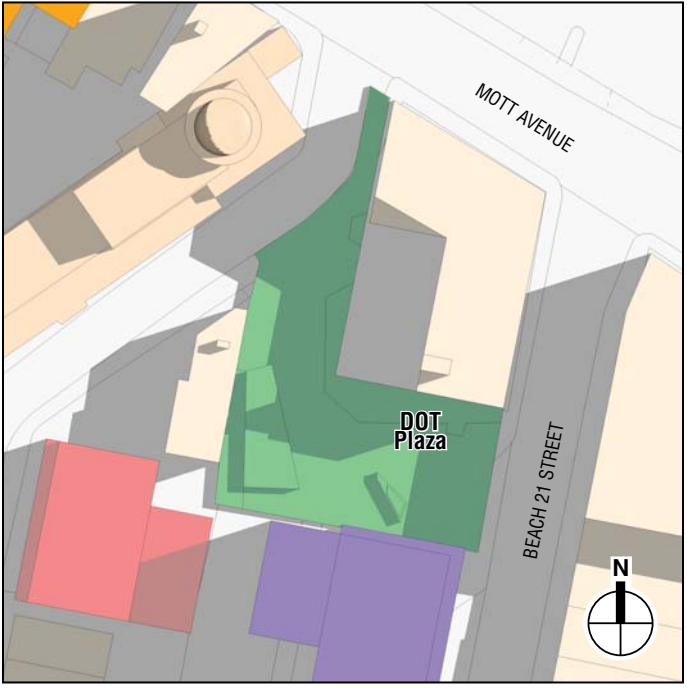
- DEIS RWCDs - Incremental Shadow
- A-Application Amended RWCDs - Additional Incremental Shadow

- Disposition Site Building
- Potential Development Site Building
- Projected Development Site Building
- Future Public Open Space Resource



NOTE: All times are Eastern Standard Time.

11.15.16



6:00 AM



6:30 AM



11:00 AM

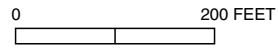


12:30 PM

- DEIS RWCDs - Incremental Shadow
- A-Application Amended RWCDs - Additional Incremental Shadow

NOTE: All times are Eastern Standard Time.

- Disposition Site Building
- Potential Development Site Building
- Projected Development Site Building
- Future Public Open Space Resource



increase to 645 feet,⁷ all sunlight sensitive resources within the longest shadow study area of Building D would fall into the triangular area to the south of Building D that could never be cast in new shadow. Therefore, with the Amended RWCDS the additional bulk added to Proposed DFRURA Building D would not result in new shadows on any sunlight-sensitive resource.

Proposed DFRURA Building E

Under the Amended RWCDS, the maximum height of Proposed DFRURA Building E would not change, but the massing of the development would be slightly altered. DEIS Figure 6-2D and 6-3D illustrate the longest shadow study area and area to the south of Building E that could never be cast in shadow. Because Redfern Houses Playground is located within the longest shadow study area and is not within the area south of the site that could never be cast in shadow, further assessment was required to determine if new shadow originating from Building E could be cast on the playground. A Tier 3 Shadow Assessment of Building E under the Amended RWCDS found that on no analysis day would the shifted bulk result in shadows long enough to reach the Redfern Houses Playground or any other sunlight-sensitive resource. Therefore, the shifted bulk of Proposed DFRURA Building E would not result in any additional incremental shadow on the Redfern Houses Playground, and would not result in additional shadow on any other sunlight-sensitive resources.

Proposed DFRURA Building F

Under the Amended RWCDS, the maximum height of Building F would not change. Therefore, the radius of the longest shadow study area disclosed in the DEIS would also remain the same. As concluded in the DEIS, no sunlight-sensitive resources intersect the longest shadow study area of a 120-foot building located on the site of Building F (see DEIS Figure 6-2F). The changes to Building F under the Amended RWCDS would not result in new shadows on any sunlight-sensitive resource.

Proposed DFRURA Building G

Under the Amended RWCDS, Proposed DFRURA Building G would reach a maximum height of 105 feet above grade, 10 feet taller than the maximum height assessed in the DEIS. The longest shadow study area of a 105-foot tall building located on the site of Building G would reach 452 feet. Because there are no sunlight-sensitive resources within 452 feet, the increased height of Proposed DFRURA Building G under the Amended RWCDS would not result in new shadows on any sunlight-sensitive resource.

Proposed DFRURA Building H

Under the Amended RWCDS, the massing of Proposed DFRURA Building H would change from what was analyzed in the DEIS. However, the building envelope of Building H under the Amended RWCDS would not increase. Therefore, the changed massing could not create new shadow beyond what was disclosed in the DEIS, and the Amended RWCDS would not result in new shadows originating from Proposed DFRURA Building H on any sunlight-sensitive resource.

⁷ According to the *CEQR Technical Manual*, the longest shadow that a structure can cast at the latitude of New York City is equal to 4.3 times the height of the structure.

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Projected Development Sites 18 and 19

For the Tier 1 assessment, the longest shadow that the Projected Development Sites 18 and 19 could cast is calculated, and, using this length as the radius, a perimeter is drawn around each structure. Anything outside this perimeter representing the longest possible shadow could never be affected by project generated shadow, while anything inside the perimeter needs additional assessment.

The building envelopes of both Projected Development Sites 18 and 19 would allow for a structure up to 95 feet above grade and maximum shadow lengths of 409 feet (95 x 4.3). **Figure 12** illustrates the longest shadow study area of Projected Development Sites 18 and 19 and the surrounding street layout. No sunlight-sensitive resources are located within the longest shadow study areas of either Projected Development Sites 18 or 19. Therefore, no further shadow assessment is required for these sites; their addition would not result in any new shadows cast on sunlight-sensitive resources.

Overall, the Amended RWCDs would not change the conclusion in the shadow assessment of the DEIS. Neither the Proposed Actions nor the Actions proposed in the A-Application would result in a significant shadow impact on any sunlight-sensitive resource.

Conceptual Analysis: Authorization to Modify Bulk Regulations

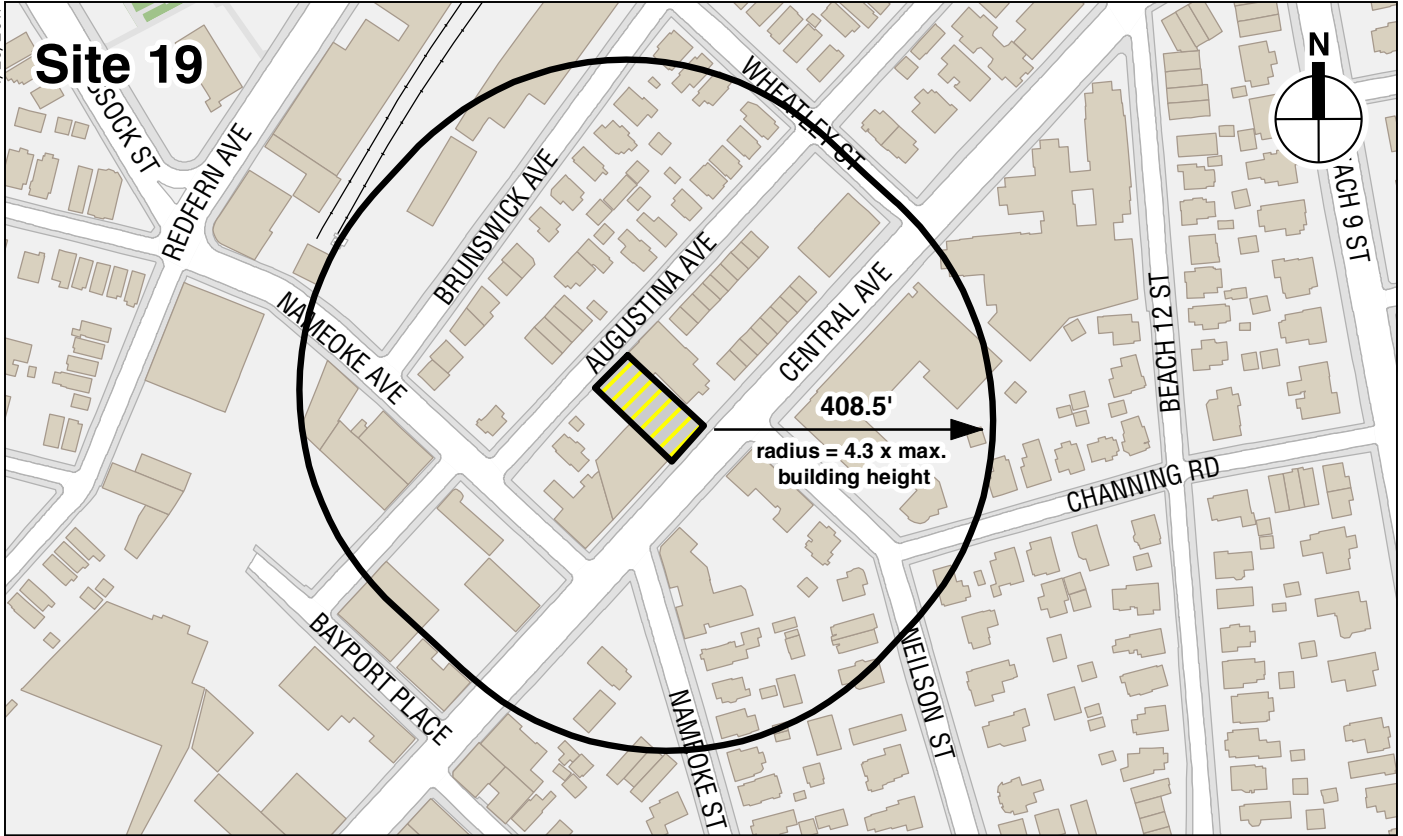
Under the Proposed Actions with the A-Application, CPC may authorize modifications of bulk regulations—including height and setback, and minimum required distance between buildings—in order to facilitate better site planning. Projected or Potential Development Sites that obtain such authorization could develop buildings that have a different form as compared to the massing analyzed under the Amended RWCDs, which in turn could result in shadowing effects that differ from those analyzed in the DEIS.

As noted above, a conceptual analysis is provided for shadows that considers the potential modifications to the design of Projected Development Site 9. **Figure 13** illustrates the achievable building design and massing for Projected Development Site 9 without authorization to modify bulk regulations, and with the authorization. Specific to Site 9, without authorization the building is approximately 100 feet in height, and without authorization it is approximately 90 feet in height. According to the *CEQR Technical Manual*, the longest shadow that a structure can cast at the latitude of New York City is equal to 4.3 times the height of the structure. The closest sunlight-sensitive resource to Projected Development Site 9 is the Cornell Burial Ground, located approximately 470 feet to the south. Therefore, Site 9 could accommodate a structure up to approximately 109 feet (470/4.3) before shadows could potentially reach a sunlight-sensitive feature.

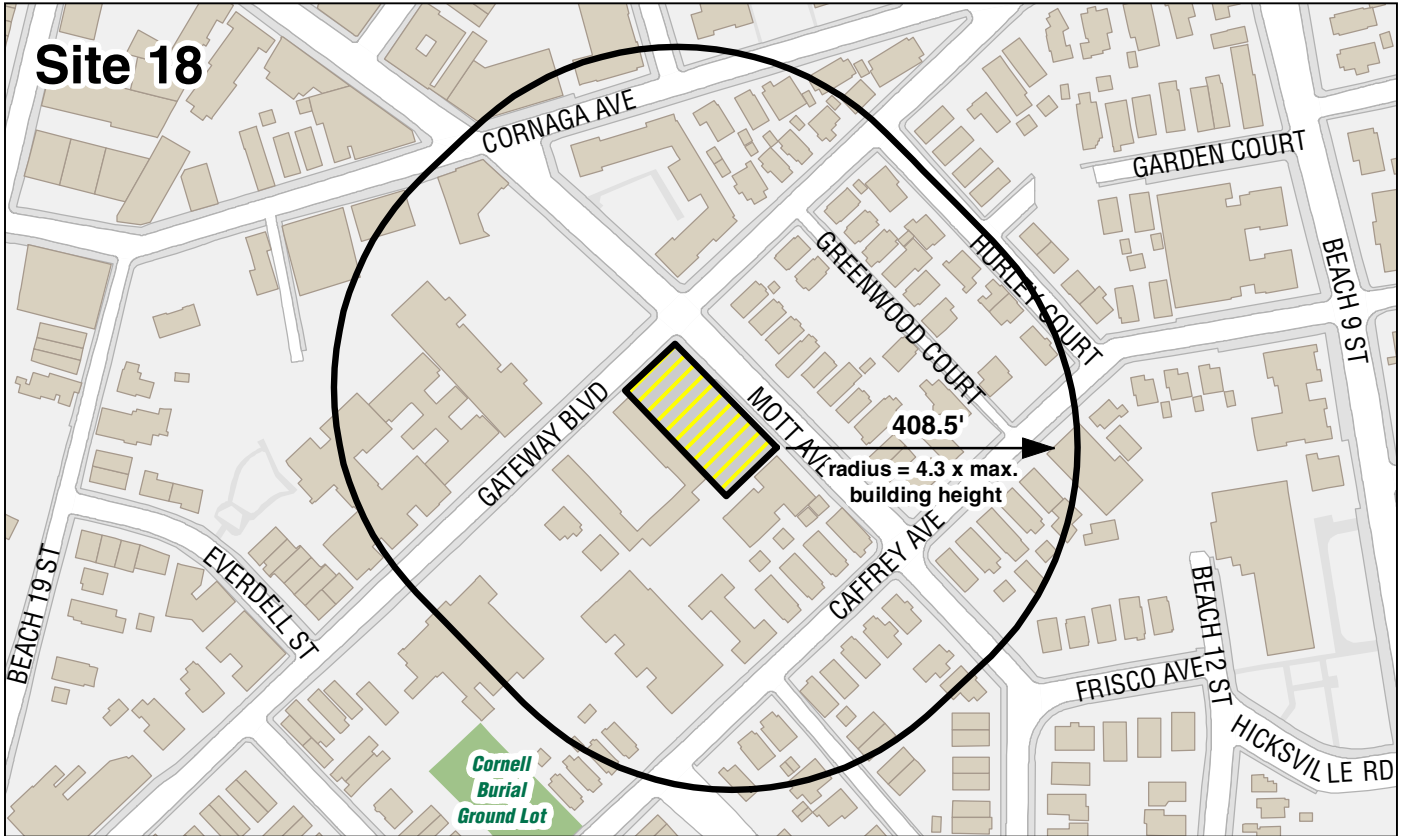
Application of the Authorization to other development sites within the Special District would be expected to generate similar results. The detailed shadows analysis in the DEIS does not indicate that any publicly accessible open space impacts would be expected to occur from minor changes in the bulk of buildings. In addition, modifications to bulk regulations would require CPC authorization, which is a discretionary action. As such, CPC findings would require that modification of bulk regulations would not obstruct access of adequate light and air to the detriment of people using the public streets and other public spaces. Therefore, the zoning text amendment authorizing modifications of bulk regulations would not result in any new or different shadow impacts not already identified in the DEIS.

4/20/2017

Site 19



Site 18



-  A-Application Projected Development Sites
-  Longest Shadow Study Area

0 400 FEET

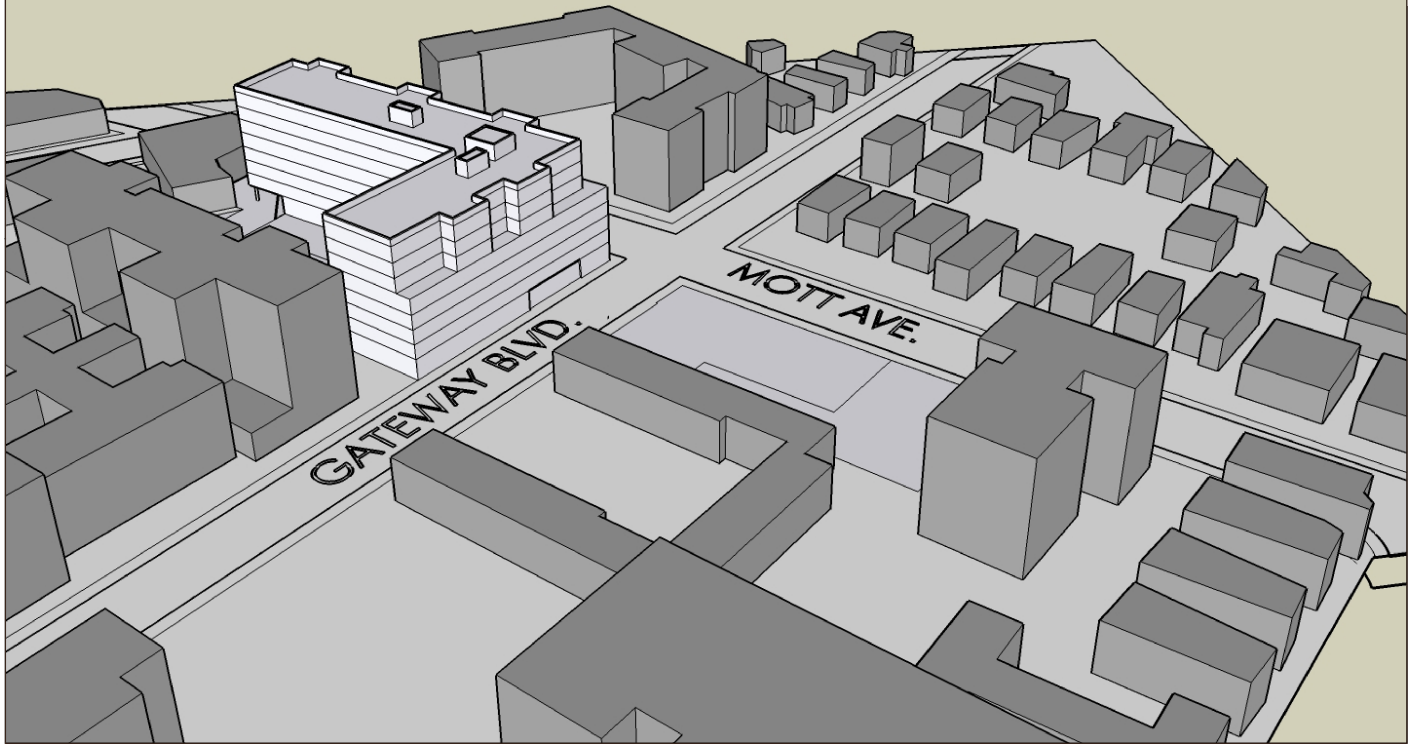


A-Application Amended
Tier 1 Shadow Assessment
Figure 12

Without Authorization



With Authorization



A-Application Amended
Projected Site 9

HISTORIC AND CULTURAL RESOURCES

As described in DEIS Chapter 7, “Historic and Cultural Resources,” the Proposed Actions would not result in significant adverse impacts to archaeological or architectural resources. Similarly, as discussed below, the A-Application would not result in any significant adverse impacts to archaeological or architectural resources.

Archaeological Resources

The study area for archaeological resources is the area where there would be increased ground disturbance as a result of the A-Application. The A-Application extends the proposed Rezoning Area to include two additional sites (Projected Development Sites 18 and Site 19), and as these two sites could experience increased ground disturbance, the study area is extended to include them. In a letter dated March 29, 2017, LPC concluded that the two additional sites have no archaeological significance (see **Appendix D**). Therefore, no further assessment of the two additional sites is warranted, and the A-Application would not result in any significant adverse impacts to archaeological resources.

Architectural Resources

The Amended RWCDS would not result in changes to development, or new development not already considered in the DEIS on any site that currently contains a known architectural resource or is adjacent to a known or potential resource. The extension of the Rezoning Area to include Projected Development Site 18 and Projected Development Site 19 would facilitate the construction of two new buildings on sites that are currently vacant lots and an older, brick 1-story building that does not meet State/National Registers of Historic Places (S/NR) eligibility.

The zoning text amendments would allow for the new development to be slightly different in appearance, but with the scale and massing of the buildings within the DFRURA to remain similar to the Proposed Actions analyzed in the DEIS. Similar to the conditions described in the DEIS, the historic resources are located within close proximity to Projected Development Sites or Potential Development Sites, and the developments resulting from the Proposed Actions could alter the setting or visual context of these historic resources, but none of the alterations would be significant adverse impacts. The Amended RWCDS would not alter the relationship of any identified historic resources to the streetscape, since all streets adjacent to historic resources would remain open and each resource’s relationship with the street would remain unchanged in the Amended With-Action Condition. Additionally, no incompatible visual, audible, or atmospheric elements would be introduced by the Amended RWCDS to any historic resource’s setting.

As described in the DEIS, one architectural resource, the S/NR-listed Trinity Chapel at 18-74 Mott Avenue, is located within 90 feet of Projected Development Site 10 and thus could potentially experience accidental damage from adjacent construction. However, DOB *TPPN #10/88* would apply to the listed building, and the building would be afforded protection by requiring a monitoring program to reduce the likelihood of construction damage and to detect at an early stage the beginnings of damage so that construction procedures can be changed. This conclusion would remain with the Amended RWCDS.

The DEIS concluded that the Proposed Actions would create incremental shadows on one historic resource, Trinity Chapel, but would not result in significant adverse shadow impacts to the building’s original stained glass window. The Amended RWCDS would not result in any

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additional shadows cast on Trinity Chapel, beyond those already disclosed in the DEIS, and would not change the impact conclusion in the shadow assessment of the DEIS.

URBAN DESIGN AND VISUAL RESOURCES

The DEIS concluded that the Proposed Actions would not result in significant adverse impacts with respect to any of the elements of urban design: streets, buildings, open space, natural features, and view corridors and visual resources. Under the Amended RWCDs, the overall height of most of the buildings within the Proposed DFRURA would be lowered, the bulk of the massings shifted to the center of the Proposed DFRURA, and the number of front and side setbacks on each building reduced. These alterations would result in buildings that would be more in keeping with the urban design of the Primary Study Area.

Along Mott Avenue, the base height of the buildings would be lowered to five stories, which would be more in keeping with the height of existing buildings along Mott Avenue and would contribute to a consistent streetwall. Additionally, Building C at the corner of Redfern and Mott Avenues would be set back from Mott Avenue an additional three feet, increasing the size of the sidewalk at this major intersection directly across Mott Avenue from the NYC MTA subway station entrance and providing better pedestrian access to the proposed DFRURA.

Compared to the DEIS conditions, the bulk of the building heights within the Proposed DFRURA would be shifted toward the middle of the DFRURA site. This would minimize the effect on the pedestrian experience by allowing the buildings along existing streets to be shorter and similar to existing buildings within the Primary Study Area. Taller buildings would continue to be grouped together and oriented toward the new north-south streets in the center of the Proposed DFRURA, with shorter buildings located closer to Mott, Central, and Redfern Avenues.

The depth of the setbacks on the buildings would be increased and the overall amount of setbacks reduced. This would allow for buildings that, at the pedestrian level on existing streets, appear similar to existing buildings and complement the existing urban design in the Primary Study Area. Along Redfern Avenue, the buildings would be approximately 40 feet tall (between three to four stories) and taller towers would rise from the base height along a new street aligned within the proposed DFRURA.

The extension of the Rezoning Area would accommodate two new Projected Development Sites not analyzed in the DEIS—Projected Development Sites 18 and 19. The inclusion of these two sites would replace vacant lots and 1-story buildings with new, taller buildings that would be constructed out to the lot line. The approximately 7-story and 9-story buildings, respectively, would activate the underutilized lots and enhance the pedestrian experience along Gateway Boulevard and Mott Avenue, and along Central Avenue north of Nameoke Street (see **Figures 5 and 6**). The overall size of the buildings would be similar to existing buildings and other Projected Development Site buildings under the DEIS RWCDs.

As described in the DEIS, views within the Primary Study Area are limited by the existing street pattern and street trees. While the buildings under the Amended RWCDs would be taller than many of the existing buildings, the lower base heights and larger greater setback depth would maintain the existing streetwalls which would not affect existing views. In particular, lowering the base height of Proposed DFRURA buildings along Mott Avenue would retain the existing limited views, including views toward Trinity Chapel, the visual resource within the Primary

Study Area. Therefore, as with the Proposed Actions, the A-Application would not result in any significant adverse impacts to view corridors or views to visual resources.

Overall, as with the Proposed Actions, the Actions proposed in the A-Application would not have a significant adverse impact on the urban design and visual resources of the study area. As compared to the Proposed Actions analyzed in the DEIS, the shifting of the bulk of the buildings on the DFRURA toward the center of the site would improve the pedestrian experience by constructing shorter buildings along existing streets.

Conceptual Analysis: Authorization to Modify Bulk Regulations

As described above, the purpose of the zoning text amendment to authorize bulk modification is to allow for better site planning. For example, as applied to Projected Development Site 9, authorization of bulk modification could result in a building that has larger floor plates and fewer setbacks along Gateway Boulevard, and would be set on a five story base with and without the bulk modification authorization (see **Figure 13**). In this instance, the building with the bulk modification authorization would better complement the urban design of the surrounding area, which consists of buildings that do not have many front or side yard setbacks.

Modifications to bulk regulations, including height and setback, yard, and distance between buildings would require CPC authorization, which is a discretionary action. The zoning text amendment would specifically state that in order to grant authorization, CPC findings would require that the modifications would provide a better distribution on the zoning lot, resulting in a superior site plan, in which the buildings subject to the authorization and any associated open areas would will relate harmoniously to one another and with adjacent buildings and open areas. In addition, as part of its authorization CPC could prescribe appropriate conditions and safeguards to minimize adverse effects on the character of the character of the surrounding area.

Therefore, consistent with the findings in the DEIS, the zoning text amendment authorizing modifications of bulk regulations would not result in significant adverse urban design impacts.

NATURAL RESOURCES

In Chapter 9 “Natural Resources,” the DEIS reported that the Proposed Project would not result in significant adverse impacts to natural resources. Similarly, the proposed A-Application would not result in significant adverse impacts to natural resources.

Projected Development Sites 18 and 19 fall outside of the existing and projected 100-year floodplain (Zone AE; the area with a one percent probability of flooding each year) and the 500-year floodplain (Zone X; the area with a 0.2 percent probability of flooding each year). Therefore, the A-Application would not result in significant adverse impacts to floodplains within the amended Project Area.

Groundwater in Queens is not used as a source of potable water. Therefore, the A-Application would not have the potential to affect drinking water supplies. The A-Application would include additional below-grade elements, including stormwater detention systems, which would have the potential to modify groundwater flow patterns. However, groundwater would be expected to flow around the walls and continue on the original direction of flow. In addition, with the implementation of measures, such as (E) designations and health and safety plans detailed under “Hazardous Materials,” significant adverse impacts to groundwater are not expected to occur due to the A-Application. Dewatering activities for construction would require treatment of the groundwater before discharge to the municipal sanitary or storm sewer in accordance with DEP

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and NYSDEC requirements and would not have the potential to adversely affect groundwater. Therefore, the proposed A-Application would not result in significant adverse impacts to groundwater.

Projected Development Site 18 is currently vacant, comprising primarily pavement or mowed lawn with few planted shrubs and trees. Projected Development Site 19 is currently occupied by a commercial building and pavement. Vegetation on Projected Development Site 19 is limited. As such, the ecological communities on these two sites consist of paved road/paths, urban structure exteriors, and mowed lawns with trees. These ecological communities provide limited habitats to wildlife other than species common to urban areas. Loss of these habitats due to development on Project Development Sites 18 and 19 may adversely affect individual wildlife unable to find suitable available habitats in the vicinity of the study area. Loss of individuals of these common species would not result in significant adverse impacts to populations of these species within the New York City metropolitan region.

The A-Application could result in the introduction of landscaped area and private open space for residents on Projected Development Site 18. The landscaped area and private open space has the potential to provide habitat for wildlife, including pollinator species and nesting birds. Landscaping such as street tree plantings has the potential to improve ecological communities and habitats for wildlife. Therefore, the A-Application has the potential to improve conditions for wildlife and vegetation in the Project Area.

There are no federal- or state-listed endangered, threatened, and special concern species, or significant natural communities considered to have the potential to occur or are known to occur within Projected Development Sites 18 and 19. Therefore, the Actions proposed in the A-Application would not result in significant adverse impacts to threatened, endangered, and special concern species and significant natural communities.

HAZARDOUS MATERIALS

The A-Application does not affect changes to the hazardous materials impact analysis in the DEIS, with the exception of the addition of Projected Development Sites 18 and 19. A hazardous materials screening has been conducted for these additional sites. This screening was similar in scope like the one conducted for Projected Development Sites analyzed in the DEIS, and included: a visual inspection of each site from public rights-of-way and surrounding area (conducted as part of the Project Area reconnaissance on May 19, 2016); review of available records and historical maps; and an evaluation of federal and state environmental regulatory databases. The hazardous materials screening identified the following:

Projected Development Site 18

At the time of the reconnaissance, this site consisted of a small plaza that is not accessible to the public, and a fenced playground, both with paved and landscaped areas. The following potential sources of contamination in close proximity to the site were identified:

- The regulatory database identified a 5,000-gallon No. 2 fuel oil underground storage tank (UST) and a closed-status spill with subsurface contamination (Spill No. 0907677) on the south-adjacent property at 14-09 Gateway Boulevard. A property at 13-24 Caffrey Avenue, approximately 125 feet south of Site 18, was identified with two No. 2 fuel oil USTs (1,500 and 3,000 gallons), and a closed-status 150-gallon release of No. 2 fuel oil due to overflow (Spill No. 8908672).

- The regulatory database listed two nearby dry cleaners as generators of hazardous waste (spent halogenated solvents). The dry cleaners were located at 14-22 Cornaga Avenue, approximately 380 feet northwest of Site 18, and at 18-17 Mott Avenue, approximately 490 feet northwest of Site 18. One of these facilities (14-22 Cornaga Avenue) was observed during the reconnaissance.

Projected Development Site 19

At the time of the reconnaissance, this site consisted of a one-story building containing a supermarket and a laundromat (with no evidence of dry cleaning) fronting Central Avenue, and a paved storage area for the building fronting Augustina Avenue. The following potential sources of contamination on or adjacent to the site were identified:

- 55-gallon plastic drums with unknown contents were noted in the storage area, with no evidence of a release observed.
- The on-site commercial building (13-20 through 13-26 Central Avenue) was built between 1912 and 1933, with a 1933 historical Sanborn map indicating that the building foundations were filled with ash. The 1951 Sanborn map showed a carpentry shop in this building.
- A closed-status spill (Spill No. 1208364) of approximately 30 gallons of transformer oil was reported for a pole-mounted electrical transformer at 13-24 Central Avenue (in front of Site 19). According to the listing, the spill may in fact have occurred at the intersection of Central Avenue and Beach 12th Street, approximately 830 feet northeast of Site 19, in which case the spill is unlikely to have affected this site.
- An auto repair shop with an approximately 275-gallon waste oil AST was observed approximately 110 feet south of Site 19.

The hazardous materials screening identified potential sources of contamination on, or in close proximity to, Projected Development Sites 18 and 19, including suspected fill materials containing ash, and nearby petroleum storage, auto repair and/or dry cleaning. To reduce the potential for adverse impacts associated with new construction resulting from A-Application actions, further environmental investigations will be required at these sites. To ensure that these investigations are undertaken, hazardous materials (E) designations would be placed on both sites.

These (E) designations require the owners of the properties to do the following prior to obtaining DOB permits for new development entailing soil disturbance or for changes to a more sensitive building use (e.g., from non-residential to residential):

- Conduct a Phase I Environmental Site Assessment (ESA) in accordance with the American Society of Testing Materials (ASTM) E1527-13, where one was not previously conducted or where required by the Mayor's Office of Environmental Remediation (OER) based on the date of the previous assessment;
- Prepare and implement a soil and groundwater testing protocol approved by OER;
- Where appropriate, conduct remediation in accordance with an OER-approved Remedial Action Plan (RAP) and Construction Health and Safety Plan (CHASP) to the satisfaction of the OER; and
- Prepare a post-construction Remedial Closure Report (RCR) documenting compliance with the RAP/CHASP, to obtain a Notice of Satisfaction and Certificates of Occupancy for newly constructed structures.

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The hazardous materials screening also identified the potential for hazardous materials in existing buildings (such as asbestos-containing materials [ACM], lead-based paint [LBP], and polychlorinated biphenyl [PCB]-containing equipment and lighting fixtures). Regulatory requirements for maintenance and (if necessary) disposal of such materials prior to or during demolition would continue to be followed.

With the implementation of the measures required by the (E) designations, the Actions proposed in the A-Application would not result in any significant adverse impacts with respect to hazardous materials.

WATER AND SEWER INFRASTRUCTURE

This section presents the expected demand for water and wastewater generation resulting from the Amended RWCDS program as compared to the RWCDS analyzed in the DEIS, and evaluates the potential for the Amended RWCDS to result in significant adverse impacts on the City's water supply and its wastewater and stormwater conveyance and treatment infrastructure.

Sewer Conveyance System

This section discusses the existing sanitary and storm sewer infrastructure serving the two new Projected Development Sites that are included in the Amended RWCDS (Projected Development Sites 18 and 19), and assesses whether any sewer system improvements would be needed to provide service to the new sites in addition to the required improvements identified in the DEIS. As discussed in DEIS Chapter 11, "Water and Sewer Infrastructure," the Project Area is located in a part of New York City served by a separated sewer system, and a review of available DEP mapping has determined that the sewer system is incomplete. In particular, several Projected Development Sites were identified in the DEIS as currently lacking an available storm sewer connection, and are not expected to be served by planned DEP improvements; therefore, further improvements would be required as part of the development on those sites.

Projected Development Site 18 is located across the street from Projected Development Site 9 in Drainage Area 2⁸ with frontages on Gateway Boulevard and Mott Street. Based on a review of DEP records, sanitary and storm sewers extend along Mott Avenue near Projected Development Site 18. Projected Development Site 19 is a through-block site located across the street from Project Development Site 14, with frontages on Central Avenue and Augustina Avenue. As discussed in the DEIS, sanitary and storm sewers extend along Central Avenue in this portion of the Project Area. Therefore, existing sewer infrastructure is available for the two additional projected developments in the Amended RWCDS.

Water Demand

Table 11 presents the projected water demand of the Amended RWCDS following the methodology outlined in the DEIS. As shown in **Table 5** above, as compared to the DEIS RWCDS, the Amended RWCDS has additional 96 additional DUs; 11,660 gsf more retail; and 6,000 gsf less community facility space.

⁸ As discussed in the DEIS, the Project Area is located in two drainage areas, with a boundary generally located along Cornaga Avenue. Under the amended RWCDS, Projected Development Sites 9 and 18 are the only Project Sites located in Drainage Area 2.

**Table 11
Proposed Project Water Consumption and Sewage Generation
Amended With-Action Condition**

Use	Size/Population	Rate	Consumption (gpd)
Drainage Area 1			
Residential Domestic	7,933 residents ¹	100 gpd/person	793,300
Residential Air Conditioning	2,947,654 gsf	0.17 gpd/sf	501,101
Retail Stores Domestic	251,687 gsf	0.24 gsd/sf	60,405
Retail Stores Air Conditioning	251,687 gsf	0.17 gpd/sf	42,787
Community Facility Domestic ²	85,947 gsf	0.10 gpd/sf	8,595
Community Facility Air Conditioning ²	85,947 gsf	0.17gpd/sf	14,611
Drainage Area 1 Water Supply Demand			1,420,799
Drainage Area 1 Sewage Generation			862,300
Drainage Area 2			
Residential Domestic	552 residents ³	100 gpd/person	55,200
Residential Air Conditioning	203,814	0.17 gpd/sf	34,648
Retail Stores Domestic	8,000	0.24 gsd/sf	1,920
Retail Stores Air Conditioning	8,000	0.17 gpd/sf	1,360
Drainage Area 2 Water Supply Demand			93,128
Drainage Area 2 Sewage Generation			57,120
Total Project Area Water Supply Demand			1,513,927
Total Project Area Sewage Generation			919,420
Source:	Rates from <i>CEQR Technical Manual</i> , Table 13-2.		
Notes:	1. Estimate based on applying the average household size for Queens Community District 14 (2.71) to 2,927 residential units in Drainage Area 1. 2. Community facility estimates use <i>CEQR Technical Manual</i> rates for commercial office space. 3. Estimate based on applying the average household size for Queens Community District 14 (2.71) to 210 residential units in Drainage Area 2.		

In total, with the Amended With-Action Condition the Proposed Project would result in projected water demand of approximately 1,513,927 gallons per day (gpd) in the Project Area, an approximately 3 percent increase in water demand as compared to the RWCDS analyzed in the DEIS (see **Table 11-4** of the DEIS). As with the RWCDS analyzed in the DEIS, this is a large increase in demand for water in the Project Area, and DEP has determined that the water mains in the Project Area would need to be upgraded. The Amended RWCDS would not alter the findings of the DEIS relating to water supply.

Sanitary Sewage Flows

As shown on **Table 11**, under the Amended With-Action Condition the Proposed Project would result in a projected sanitary sewage generation of approximately 919,420 gpd; this includes approximately 862,300 gpd in Drainage Area 1, and approximately 57,120 gpd in Drainage Area 2. In total, sanitary sewage generation would increase by approximately 3 percent as compared to the RWCDS analyzed in the DEIS (see **Table 11-4** of the DEIS), with the largest increase occurring in Drainage Area 2 as a result of the new development on Projected Development Site 18.

As with the DEIS RWCDS, this increase in sanitary sewage generation in the Project Area would not present a significant increase in average daily flow to the Rockaway Wastewater Treatment Plant (WWTP), which serves both Drainage Areas 1 and 2, and would not result in an exceedance of the plant’s permitted capacity of 45 million gallons per day (mgd) or otherwise affect the plant’s treatment efficiency. As discussed in the DEIS, upgrades to sanitary sewers in the Project Area are expected to be required to support the higher density development resulting from the Proposed Actions, which will be reflected in the amended drainage plan adopted by DEP. As necessary, the drainage plan amendment will be revised to include the rezoning area boundaries and development

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density as modified by the A-Application. As planned and future infrastructure improvements to support future development would be based on the amended drainage plan, any upgrades to the sanitary sewer system required in the Project Area will account for the increased demand resulting from the A-Application if adopted. Therefore, the A-Application would not alter the findings of the DEIS relating to sanitary sewage conveyance and treatment infrastructure.

Stormwater Flows

Similar to the other Projected Development Sites analyzed in the DEIS, the new developments on Projected Development Sites 18 and 19 would result in an increase in impervious surface coverage, with new fully impervious rooftop areas (particularly on Projected Development Site 18, which is currently vacant though the site would be expected to contain some pervious surfaces as part of its public open space). The additional developments would result in increased stormwater runoff volumes to the sewer system. However, as with the other developments, any new development on these sites would be required to implement stormwater best management practices (BMPs) as part of the DEP site connection approval process in order to bring the building(s) into compliance with the required stormwater release rate. These BMPs, which may include planted rooftop spaces (“green roofs”) and/or vaults, would ensure that the developments discharge at a rate that would not exacerbate the surcharged condition of the downstream storm sewers, in accordance with DEP regulations.

As noted above, storm sewers are currently available for connection in accordance with DEP regulations by the new developments on Projected Development Sites 18 and 19. As with the other developments in the Project Area, for sites that front sewers not constructed to the updated drainage plan a hydraulic analysis of the existing sewer system may be required to determine whether the existing sewer system serving the sites is capable of supporting higher density development. Therefore, the Actions proposed in the A-Application would not alter the findings of the DEIS relating to stormwater conveyance infrastructure.

SOLID WASTE AND SANITATION SERVICES

This section considers whether the Amended RWCDS could overburden available waste management capacity or otherwise be inconsistent with the city’s Solid Waste Management Plan (SWMP), or with state policy related to the city’s integrated solid waste management system.

Development facilitated by the A-Application would create solid waste and an increased demand for sanitation services. **Table 12** presents the projected total solid waste generation in the Amended With-Action Condition, based on the Amended RWCDS presented in **Table 5** above and solid waste generation rates from the *CEQR Technical Manual*. The Amended RWCDS would generate an estimated 131.03 tons of solid waste per week, which represents 104.61 additional tons in weekly solid waste generation as compared to the No Action Condition.

Of the 130.73 tons of solid waste per week generated by the Amended RWCDS, commercial uses would generate an estimated 64.0 tons. Solid waste generated by commercial uses would be collected by private commercial carters, and commercial buildings would be subject to mandatory recycling requirements for paper, metals, construction waste, aluminum foil, glass, and plastic containers. Residential and community facility uses would generate an estimated 65.68 tons of solid waste per week. Solid waste generated by residential and community facility uses would be collected by DSNY and would be served by DSNY collected routes. As a general practice, DSNY adjusts its operations to service the community. Residents would be required to participate in the City’s recycling program for paper, metals, and certain types of plastics and glass.

**Table 12
Amended With-Action Condition Solid Waste Generation on the
Proposed DFRURA, Disposition Sites, and Projected Development Sites**

Use	Floor Area (gsf)	Population	Solid Waste Generation Rate (lbs/wk)	Solid Waste Generation	
				(lbs/week)	(tons/wk)
Residential	3,131	3,131 households	41 per household	128,372	64.0
General Retail	122,052	367 employees	79 per employee	28,993	14.5
Restaurant	45,446	136 employees	251 per employee	34,136	17.07
Food Store	46,744	140 employees	284 per employee	39,760	19.88
Fast Food	45,446	136 employees	200 per employee	27,200	13.6
Community Facility	85,947	258 employees	13 per employee	3,354	1.68
Total Solid Waste Generation				262,060	130.73

Notes: With the exception of a supermarket and bank on the Proposed DFRURA and a supermarket and laundromat on Projected Development Site 19, specific retail use types resulting from the Proposed Actions are unknown; for purposes of this analysis remaining retail use types and gsf allocation are based on existing proportion of retail uses within U.S. Census Tracts 1032.01, 1032.02, 1010.01, and 1008.02, which collectively approximate the Project Area. The Restaurant and Fast Food gsf allocation assumes that businesses within the “Food Services and Drinking place” category are evenly distributed as Restaurants and Fast Food uses. Community facility solid waste generation rate is based on the “Office Building” rate in table 14-1 of the *CEQR Technical Manual* and assumes one employee per 333 gsf. Specific community uses resulting from the Proposed Actions are unknown, and therefore the waste generated by all community facility uses is assumed to be handled by DSNY.
Sources: *CEQR Technical Manual* Table 14-1; Esri Business Analyst’s *Business Summary Profile* for Census Tracts 1032.01, 1032.02, 1010.01, and 1008.02.

Table 13 shows the increment between the solid waste generated by the DEIS RWCDs and that generated by the Amended RWCDs. The incremental development associated with the Amended RWCDs would result in an additional estimated 5.5 tons per week of total solid waste. Approximately 30 percent (1.67 incremental tons) of this additional solid waste is projected to be collected from residential and community facility use sites by DSNY. The remaining 70 percent (3.84 incremental tons) of additional solid waste is projected to be collected from commercial use sites by private carters.

**Table 13
Comparison of Weekly Solid Waste Generation in Tons
DEIS RWCDs Compared to Amended RWCDs**

	DEIS RWCDs Increment (tons/week)	Amended RWCDs Increment (tons/week)	Difference (tons/week)
Total solid-waste generation	125.23	130.73	5.5
Solid waste handled by DSNY	64.01	65.68	1.67
Solid waste handled by Private Carters	61.21	65.05	3.84

As shown in **Table 14**, relative to the No Action condition, the Amended With-Action Condition would result in an approximately 64.0-ton increase in weekly solid waste handled by DSNY. This increment would represent approximately 0.06 percent of the City’s anticipated future solid waste generation handled by DSNY (it is estimated that DSNY will manage 115,830 tons of

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solid waste for export, recycling compost and refuse per week by 2026), as projected in the 2006 SWMP.⁹ Based on the typical DSNY collection truck capacity of approximately 12.5 tons, the new residential and community facility uses introduced by the A-Application would be expected to generate additional solid waste equivalent to approximately 5.26 truckloads per week. This would not be expected to overburden DSNY’s solid waste handling services.

Table 14

**Comparison of Weekly Solid Waste Generation in Tons
in the Amended With-Action Condition**

	No Action Condition (tons/week)	Amended With-Action Condition (tons/week)	Amended With-Action Increment over No Action Condition (tons/week)
Total solid-waste generation	26.42	130.73	104.31
Solid waste handled by DSNY	0.26	65.68	65.42
Solid waste handled by Private Carters	26.16	65.05	38.89

Also shown in **Table 14**, compared to the No Action condition, the Amended RWCDs would result in an approximately 38.89-ton increase in weekly solid waste handled by private carters. This would represent approximately 0.05 percent of the City’s anticipated future commercial waste generation, as it is estimated that private carters will carry 74,000 tons of solid waste per week by 2025, as projected in the SWMP.¹⁰ Based on the typical commercial truck capacity of between 12 and 15 tons of waste per truck, development in the Amended With-Action Condition would require between 4.34 and 5.42 private carter collection trucks per week, or an increment of between 2.59 and 3.24 additional private carter collection trucks per week as compared to the No Action condition. There are more than 2,000 private carting businesses authorized to service NYC, and it is expected that their collection fleets would be sufficiently flexible to accommodate this increased demand for solid waste collection. Therefore the incremental commercial solid waste handled by private carters would not overburden the City’s waste management system.

Overall, the A-Application would not conflict with the SWMP, or have a direct on a solid waste management facility. The incremental solid waste generated in the Amended With-Action Condition would not overburden the City’s solid waste handling system, and therefore the A-Application Actions would not have a significant adverse impact on the City’s solid waste and sanitation services.

ENERGY

This section estimates the amount of energy that would be consumed annually as a result of day-to-day operation of buildings and uses resulting from the Amended RWCDs. The *CEQR Technical Manual* recommends a detailed analysis of energy impacts for projects that could significantly affect the transmission or generation of energy. Most actions resulting in new construction, including the Proposed Actions as updated by the A-Application, would not create significant energy impacts, and

⁹ Comprehensive Solid Waste Management Plan, September 2006; Attachment II, Table 2-6, p. 25. Accessed April 3, 2017

¹⁰ Comprehensive Solid Waste Management Plan, September 2006; Attachment II, Table IV 2-2, p. 4. Accessed April 3, 2017

as such do not require a detailed energy assessment. However, following *CEQR Technical Manual* guidelines, a project’s operational energy consumption should be estimated.

Development facilitated by the A-Application would create an increased demand on energy systems, including electricity and gas. **Table 15** presents the projected future energy consumption in the Amended With-Action Condition, based on the Amended RWCDs presented in **Table 5** above and energy consumption rates in the *CEQR Technical Manual*. Consistent with assumptions used in the DEIS, for analysis purposes, the Amended RWCDs residential uses are assumed to consume energy at the large residential building type rate (126.7 MBTU/year), with the exception of two small residential buildings that would be developed on the DSNY Disposition Site, which are assumed to consume energy at the small residential building type rate (94.0 MBTU/year). Retail space is assumed to consume energy at the commercial use rate (216.3 MBTU/year), and community facility space is assumed to consume energy at the institutional use rate (250.7 MBTU/year).

Table 15
Projected Future Energy Consumption Amended With-Action Condition

Use	Size ¹ (gsf)	Rate (MBTUs/gsf/year)	Energy Consumption (MBTUs/Year)	Incremental Annual Energy Use (MBTU) over No Action Conditions
Commercial ²	259,687	216.3	56,170,211.6	+ 36,458,360.0
Industrial ³	0	554.3	0	- 24,290,534.6
Institutional ⁴	85,947	250.7	21,546,912.9	+ 20,293,412.9
Large Residential ⁵	3,157,367	126.7	399,290,955.6	+ 398,322,374.1
Small Residential ⁶	8,000	94	0	+ 718,630.0
Total Energy Consumption			477,008,120.1	+ 430,064,982.4

Notes: gsf = gross square feet
¹ GSF amounts reflect total development in the With-Action Condition, not incremental development that discounts for existing uses on development sites.
² Includes retail, supermarket, and restaurants.
³ There are no manufacturing facilities, factories, auto-related, or storage/garage users anticipated with the Proposed Actions.
⁴ Includes community facilities.
⁵ Includes residential buildings with more than 4 dwelling units.
⁶ Includes residential buildings with 1-4 dwelling units.
Source: 2014 *CEQR Technical Manual*, Table 15-1, “Average Annual Whole-Building Energy Use in New York City.”

As shown in **Table 15**, the Amended RWCDs would demand an estimated 447 billion BTUs of energy annually, which represents a net increase of approximately 430 billion BTUs over No Action conditions. This net increase in annual demand would represent about 0.001 percent of Long Island Power Authority (LIPA) service area’s (Zone K) forecasted annual energy requirement of 78.69 trillion BTUs for 2025, and therefore, is not expected to result in a significant adverse impact on energy systems.

Additionally, new development as a result of the A-Application would be required to comply with New York City Energy Conservation Code (NYCECC), which governs performance requirements of heating, ventilation, and air condition systems, as well as the exterior building envelope of new buildings. In compliance with this code, new developments must meet standards for energy conservation, which include requirements relating to energy efficiency and combined thermal transmittance. In addition, should there be a voluntary utilization of higher performance standard designs on development sites, this would lead to a reduction in the energy load forecasted in **Table 15**.

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The increment between the future energy consumption projected in the DEIS and the future energy consumption projected for the Amended With-Action Condition is displayed in **Table 16**. As shown, the Amended RWCDs would result in an estimated 13,248,198.3-MBTUs increase over the DEIS’s projected energy consumption, a 3.4 percent increase that would not materially affect the DEIS findings.

Table 16
Increment of Projected Future Energy Consumption of DEIS RWCDs and Amended RWCDs

Use	DEIS RWCDs Energy Consumption (MBTUs/Year)	Amended RWCDs Energy Consumption (MBTUs/Year)	Amended RWCDs Incremental Annual Energy Use (MBTU) over DEIS RWCDs
Commercial ¹	52,748,432.1	56,170,211.6	+ 3,421,779.5
Industrial ²	0.0	0.0	0
Institutional ³	23,051,112.9	21,546,912.9	- 1,504,200.0
Large Residential ⁴	387,208,376.8	399,290,995.6	+ 12,082,618.8
Small Residential ⁵	752,000.0	0.0	-752,000.0
Total Energy Consumption	463,759,921.8	477,008,120.1	+ 13,248,198.3

Notes: gsf = gross square feet
¹ Includes retail, supermarket, and restaurants.
² There are no manufacturing facilities, factories, auto-related, or storage/garage users anticipated with the Proposed Actions.
³ Includes community facilities.
⁴ Includes residential buildings with more than 4 dwelling units.
⁵ Includes residential buildings with 1-4 dwelling units.
Source: 2014 CEQR Technical Manual, Table 15-1, “Average Annual Whole-Building Energy Use in New York City.”

TRANSPORTATION

The Amended RWCDs would generate 110, 252, 202, and 210 additional person trips in the weekday AM, midday, PM and Saturday midday/afternoon peak hours, respectively (see detailed travel demand forecast in **Appendix E**).

Traffic

As summarized in **Table 17** and as shown in **Appendix E**, the Amended RWCDs would result in 29, 28, 36, and 34 additional vehicle trips in the weekday AM, midday, PM and Saturday midday/afternoon peak hours, respectively.

Table 17
Additional Vehicle Trips associated with the Amended RWCDs

Revised Site 9 (Net Increase)			Site 18			Site 19			Total Additional Trips		
In	Out	Total	In	Out	Total	In	Out	Total	In	Out	Total
0	0	0	4	18	22	1	6	7	5	24	29
6	6	12	5	5	10	3	3	6	14	14	28
3	3	6	15	7	22	5	3	8	23	13	36
2	2	4	10	10	20	5	5	10	17	17	34

These trips would be dispersed throughout the network and would therefore result in small increases of traffic volumes at a portion of the analyzed intersections. In coordination with DOT, intersections with a level of service worse than mid-LOS D in the With-Action Condition presented in the DEIS and that would experience an increase in traffic volumes associated with

the Amended RWCDs were selected for a screening analysis. Intersections with a mid-LOS D or better in all four peak periods in the With-Action Condition presented in the DEIS were not considered for this analysis. The increases in delay for the lane groups affected by the additional trips associated with the Amended RWCDs are shown below in **Table 18**. A detailed LOS table is provided in **Appendix E**.

With the exception of lane groups at the intersections of Mott Avenue at Cornaga Avenue and Cornaga Avenue at Beach 9th Street/Empire Avenue where additional trips to and from Projected Development Site 18 would be most concentrated, the affected lane groups considered for this screening analysis would experience minor increases in delay which are not anticipated to result in new significant adverse traffic impacts or to require new or different mitigation measures.

At Mott Avenue and Cornaga Avenue, the DEIS identifies significant adverse impacts to several lane groups in all four analysis peak hours. As presented in the DEIS, these impacts could not be fully mitigated in all four peak hours. The southbound approach, which is identified as unmitigated in the PM peak hour in the DEIS, would become unmitigated in the weekday midday and Saturday peak hours as a result of the additional vehicle trips associated with the Amended RWCDs (refer to **Appendix E** for a detailed comparison table).

At Cornaga Avenue and Beach 9th Street, the DEIS identifies unmitigated impacts to several lane groups in one or more peak hours. The additional vehicle trips associated with the Amended RWCDs would result in a new unmitigated impact to the northbound approach in the weekday midday and PM peak hours, as indicated in **Table 18**. It should be noted that the DEIS identifies an unmitigated impact to the northbound approach in the AM peak hour and further identifies unmitigated impacts to other lane groups at this intersection in both the weekday midday and PM peak hours (refer to detailed LOS table in **Appendix E**).

Transit

The Amended RWCDs would generate 17, 22, 26, and 23 additional subway trips and 18, 18, 26, and 23 additional bus trips in the weekday AM, midday, PM and Saturday midday/afternoon peak hours, respectively.

Subway

As presented in the DEIS, the analyzed Far Rockaway-Mott Avenue A-train subway station elements (stairs and fare arrays) would operate at LOS A in the DEIS With-Action Condition in both the AM and PM analysis peak hours. It is anticipated that this LOS would not change under the Amended With-Action Condition with the additional subway trips associated with the Amended RWCDs. Similarly, the effect of these additional trips on the subway line haul conditions is expected to be minimal, and it is anticipated that the analyzed A-train would continue to operate well below capacity in the Amended With-Action Condition.

Bus

The 18 additional AM peak hour and 26 additional PM peak hour trips generated by the Amended RWCDs would be distributed among several MTA and NICE (Nassau Inter-County Express) buses, and are therefore expected to minimally affect the bus line haul conditions on analyzed bus routes. It is therefore anticipated that the measures identified in the DEIS to mitigate the significant adverse impact to westbound Q22 service in the AM peak hour and to eastbound Q22 service would also fully mitigate these bus impacts under the Amended With-Action Condition. It should be noted that, based on comments received from the MTA on the bus analysis methodology, the northbound Q113/Q114 service would also experience a significant adverse impact. Mitigation measures to fully mitigate this impact will be presented in the FEIS.

Table 18
Increase in Delay resulting from Additional Vehicle Trips associated with the Amended RWCDS

Intersection	Lane Group	Increase in Delay: DEIS With-Action vs. A-Application			
		Weekday AM Peak Hour	Weekday Midday Peak Hour	Weekday PM Peak Hour	Saturday Peak Hour
4. Cornaga Ave at Rockaway Freeway	EB-LTR	-	-	0.1 sec.	-
	WB-LTR	-	0.2 sec.	1.0 sec.	0.1 sec.
	NB-TR	-	0.2 sec.	0.2 sec.	0.1 sec.
5. Cornaga Ave at Beach 22 nd St	EB-TR	-	0.1 sec.	0.1 sec.	-
	WB-LT	0.1 sec.	0.1 sec.	-	-
6. Cornaga Ave at Beach 20 th St	EB-TR	-	0.1 sec.	-	-
	WB-LT	0.1 sec.	0.2 sec.	0.1 sec.	-
	SB-LTR	-	1.2 sec.	1.2 sec.	-
7. Mott Ave at Cornaga Ave	EB-LTR	0.6 sec.	-	0.2 sec.	0.1 sec.
	WB-LT	0.2 sec.	0.5 sec.	0.4 sec.	0.3 sec.
	NB-LTR	-	2.5 sec.	2.6 sec.	0.8 sec.
	SB-LTR	13.5 sec.	33.0 sec.	58.1 sec.	24.1 sec.*
8. Cornaga Ave at Beach 9 th St/Empire Ave	EB-LTR	14.7 sec	2.7 sec.	5.2 sec.	2.5 sec.
	NB-LTR	0.9 sec	1.2 sec.*	1.3 sec.*	0.2 sec
	SB-LTR	3.7 sec	1.8 sec.	12.9 sec.	1.1 sec.
9. Mott Ave at Beach Channel Drive	EB-LTR	-	0.1 sec.	-	-
10. Mott Avenue at Beach 21 st Street	NB-R	-	1.5 sec.	-	-
11. Mott Ave at Central Ave/Beach 20 th St	EB-R	-	0.7 sec.	0.4 sec.	-
	WB-LTR	-	0.1 sec.	0.3 sec	-
	SB-R	-	-	5.6 sec	-
17. Nameoke Ave at Beach Channel Drive	NB-TR	-	-	0.2 sec.	-
	SB-L	-	-	0.3 sec.	-
	SB-TR	-	-	0.5 sec.	-
19. Dix Ave at Beach Channel Drive	NB-LTR	-	-	0.2 sec.	-
	SB-LTR	-	-	0.6 sec.	-
22. Neilson St at Central Ave	NB-T	1.0 sec.	0.1 sec.	-	0.1 sec.
	SB-T	-	0.1 sec.	1.6 sec.	0.2 sec.
28. Hassock St at Beach Channel Drive	NB-LT	-	-	0.6 sec.	-
	SB-T	-	-	0.5 sec.	-

Note: * - lane group with significant adverse impact resulting from the additional vehicle trips associated with the Amended RWCDS.

Pedestrians

The Amended RWCDS would generate 70, 208, 154, and 165 additional pedestrian trips in the weekday AM, midday, PM and Saturday midday/afternoon peak hours, respectively (including walk-only trips, and trips to and from subway stations and bus stops). As presented in the DEIS, with the exception of the west crosswalk at Beach 21st Street and Mott Avenue, which would operate at LOS D and would experience significant adverse impacts in the weekday PM and Saturday peak hours, all pedestrian elements would operate at LOS C or better in all peak hours in the DEIS With-Action Condition. Due to the dispersed nature of walk-only trips and the relatively small number of additional subway and bus trips resulting from the Amended RWCDS, the effect on the LOS analysis results presented in the DEIS is expected to be minimal.

Parking

Compared to the With-Action Condition presented in the DEIS, the Amended With-Action Condition would result in an additional on-street parking demand of 51 spaces in the weekday midday period and 82 spaces in the overnight period. With this additional demand, approximately 1,674 and 45 on-street spaces would remain available within a ¼-mile of the Rezoning Area during the weekday midday and overnight peak periods, respectively. Therefore, the A-Application is not expected to result in new significant adverse parking impacts during the weekday midday peak period for commercial and retail parking demand, nor during the overnight peak period for residential demand.

AIR QUALITY

Mobile Sources

The Amended With-Action Condition would result in additional vehicle trips as presented in the Transportation section of this memorandum. The additional traffic associated with the Actions proposed in the A-Application would result in less than 2 percent increase in the project increments at the analyzed intersections compared to the DEIS. Based on the mobile source analysis presented in the DEIS for the Proposed Actions, maximum concentrations of carbon monoxide (CO) and particulate matter less than 2.5 microns in diameter (PM_{2.5}) were predicted to be well below applicable standards referenced in the *CEQR Technical Manual*. The maximum predicted 24-hour and annual average PM_{2.5} increments at the analyzed intersections are 13 percent and 66 percent of the *de minimis* threshold, respectively. The maximum predicted CO concentration is 30 percent of the *de minimis* threshold. Therefore, the additional traffic under the Amended With Action Condition would not result in any significant adverse air quality impacts. Nevertheless, the mobile source analysis would be updated and presented in the FEIS.

Stationary Sources

A screening analysis was performed for the two additional Projected Development Sites (i.e., Projected Development Sites 18 and 19) introduced in the A-Application using the methodology described in Chapter 15, “Air Quality” of the DEIS. For the Projected Development Site 19, burning No. 2 fuel oil or natural gas would not result in potential significant adverse air quality impacts because the proposed building would be below the maximum development size shown in DEIS Figures 17-5 and 17-7 of the Air Quality Appendix of the *CEQR Technical Manual*, respectively. Projected Development Site 18 failed the screening analysis using No. 2 fuel oil or natural gas as the fuel source. Therefore, a refined analysis was required for this Development Site using the EPA AERMOD model, following the methodology outlined in the *CEQR Technical Manual* and the assumptions described in the DEIS. The refined analysis was performed to determine potential air quality impacts on Projected Development Site 9, which is the nearest building of a similar or greater height. The analysis determined that a natural gas restriction, stack set back and a requirement to utilize low NO_x burners would be required to avoid any potential significant air quality impacts on Projected Development Site 9. The text of the (E) designation would be as follows:

Projected Development Site 18 (Block 15574, lot 36)

Any new residential and/or commercial development must use heat and hot water system fitted with low NO_x (30 ppm) burners firing natural gas, and ensure that the heat and hot water system stack(s) is located at least 10 feet away from the lot line facing Gateway Boulevard, to avoid any potential significant air quality impacts.

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Projected Development Site 9 would not result in any potential impacts since it would be taller in height than existing or proposed developments within 400 feet. Therefore, the Amended RWCDs program for Projected Development site 9 would not change the conclusions presented in the DEIS.

As described above, the A-Application also includes text amendments that would affect the building massing on the Proposed DFRURA. The air quality analysis presented in the DEIS determined that to preclude the potential for significant adverse air quality impacts from fossil fuel-fired heat and hot water systems, an air quality (E) designation would be assigned as part of the Proposed Actions for five of the DFRURA sites. These designations specify the various restrictions, such as type of fuel to be used, the use of low NO_x burners, the distance that the vent stack on the building roof must be from its lot line(s), and/or the increase of the exhaust stack height. With the proposed adjustments to the building massing on the URA sites, these restrictions may still be required but could be revised to ensure that the adjusted massing would not create any new air quality impacts. For the other Proposed DFRURA sites for which would be lower in height as a result of the A-Application, it is possible that these modifications would result in air quality (E) designations not reported in the DEIS. Therefore, an analysis of the revised building massing for the Proposed DFRURA will be performed and presented in the FEIS. This analysis will summarize the required air quality (E) designations.

Overall, the Actions proposed in the A-Application would not result in any significant adverse air quality impacts.

GREENHOUSE GAS EMISSIONS AND CLIMATE CHANGE

Adjustments in the building massing on the Proposed DFRURA resulting from the modifications to the Special District text proposed in the A-Application would not change the building areas or locations, and would therefore not affect any change in the DEIS analysis or conclusions regarding greenhouse gas (GHG) emissions and resilience.

The amended RWCDs assumptions for Projected Development Site 9 and the addition of Projected Development Sites 18 and 19 would result in a net increase of building floor area resulting from the Proposed Actions, including a small increase in residential and retail use and a small decrease in community facility use. This net increase in overall floor area would result in an increase in energy consumption and ensuing GHG emission associated with the construction and operation of buildings in the Amended With-Action Condition.¹¹ As described in the DEIS for other Projected Development Sites, promotion of the GHG reduction goal through improved efficiency of site-specific building systems and similar measures cannot be achieved within the scope of the Proposed Actions at sites that may be developed as a result of this action but not otherwise controlled by the City. However, if developers of any of these sites apply for United States Department of Housing and Urban Development (HUD) funding through HPD, the requirements of the Enterprise Green Communities (EGC) program to achieve a minimum of 15 percent reduction in energy expenditure relative to the current building code and other sustainability measures that would indirectly reduce GHG emissions would apply.

¹¹ This does not necessarily indicate a net increase in GHG emissions in general, since truly accounting for the incremental emissions only would require speculation regarding where people would live in a No Action condition if residential units are not built at these locations, what energy use and efficiency might be like for those alternatives and other related considerations, and similar assumptions regarding commercial uses.

Projected Development Sites 18 and 19 are not located in the existing or projected floodplain, and therefore, analysis of resilience to potential future coastal flooding events is not required.

Overall, the Actions proposed in the A-Application would not materially change the GHG emissions and climate change conclusions presented in the DEIS.

NOISE

The proposed changes to the RWCDS within the DFRURA would not result in any new or additional stationary noise sources, and would not result in changes to traffic volumes that would appreciably change the future noise levels predicted in the DEIS noise analysis. Consequently, as was concluded in the DEIS noise analysis, the Proposed Actions would not have the potential to result in any significant adverse noise impacts at any existing noise receptors. However, as with the DEIS noise analysis, noise exposure at newly introduced noise receptors under the Proposed Actions would require Noise (E) Designations to ensure acceptable interior noise levels at buildings to be constructed within the Proposed DFRURA and on Projected Development and Disposition Sites. The proposed changes to the RWCDS would not alter any of the Noise (E) Designations prescribed in Table 17-10 of the DEIS. The additional Projected Development Sites (i.e., Projected Development Sites 18 and 19) introduced in the A-Application were also analyzed to determine the required level of window/wall attenuation to ensure acceptable interior noise levels according to *CEQR Technical Manual* noise exposure guidance. As in the DEIS noise analysis, the recommended noise attenuation values for buildings are designed to maintain interior noise levels of 45 dBA or lower for residential or community facility uses and 50 dBA or lower for retail and office uses and are based on exterior L₁₀₍₁₎ noise levels. Required attenuation levels for Projected Development Sites 18 and 19 were determined based on the maximum projected L₁₀₍₁₎ noise levels shown in Table 17-9 of the DEIS. The attenuation requirements for these Projected Development Sites are shown in **Table 19**.

Table 19
Required Attenuation at Projected Development Sites 18 and 19 (in dBA)

Site Descriptor	Block	Lot(s)	Façade(s)	Representative Receptor Site	Minimum Required Attenuation ²
Projected Site 18	15574	36	All	13	N/A ¹
Projected Site 19	15535	11, 58, 59, 60, 61	East, North, South	1	31
			West	2	28

Notes:
¹ "NA" indicates that the highest calculated L₁₀ is below 70 dBA. The *CEQR Technical Manual* does not specify minimum attenuation guidance for exterior L₁₀ values below this level. Projected Development Site 18 would have no minimum required facade attenuation.
² Attenuation values are shown for residential or community facility uses; retail and office uses would require 5 dBA less attenuation.

To implement the attenuation requirements at Projected Sites, an (E) designation for noise would be applied specifying the appropriate amount of window/wall attenuation. The text of the (E) designation would be as follows:

To ensure an acceptable interior noise environment, the building façade(s) of future development on the Blocks and Lots shown in Table 19 must provide minimum composite building façade attenuation as shown in Table 19, in order to maintain an interior L₁₀ noise level not greater than 45 dBA for residential and community facility uses or not greater than 50 dBA for commercial uses. To maintain a closed-window condition in these areas, an alternate means of ventilation that brings outside air into the building without degrading the acoustical performance of the building façade(s) must also be provided.

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The attenuation of a composite structure is a function of the attenuation provided by each of its component parts and how much of the area is made up of each part. Normally, a building façade is composed of the wall, glazing, and any vents or louvers for HVAC systems in various ratios of area. Buildings proposed to be located on the (E) designated sites would be designed to provide a composite Outdoor-Indoor Transmission Class (OITC) rating greater than or equal to the attenuation requirements listed in **Table 19**. The OITC classification is defined by ASTM International (ASTM E1332-10) and provides a single-number rating that is used for designing a building façade including walls, doors, glazing, and combinations thereof. The OITC rating is designed to evaluate building elements by their ability to reduce the overall loudness of ground and air transportation noise.

By adhering to the Noise (E) Designation described above, buildings to be developed on Projected Development Sites 18 and 19 would provide sufficient attenuation to achieve the *CEQR Technical Manual* interior noise level guidelines of 45 dBA L₁₀ for residential or community facility uses and 50 dBA L₁₀ for commercial uses.

PUBLIC HEALTH

The *CEQR Technical Manual* states that a public health assessment is not necessary for most projects. Where no significant adverse unmitigated impacts are found in other CEQR analysis areas—such as air quality, water quality, hazardous materials, or noise—no public health analysis is warranted. If, however, an unmitigated adverse impact is identified in any of these other CEQR analysis areas, the lead agency may determine that a public health assessment is warranted for that specific technical area.

As described in the relevant analyses of this technical memorandum, the A-Application would not result in an unmitigated significant adverse impact in the areas of air quality, water quality, hazardous materials, or operational noise. However, as discussed in “Construction” below, like the Proposed Actions analyzed in the DEIS, the Actions proposed in the A-Application could result in unmitigated construction noise impacts.

Construction activities associated with the Amended RWCDS would have the potential to result in significant adverse noise impacts that would not be fully mitigated. Despite these potentially unmitigated construction noise impacts, the predicted overall changes in noise levels would not be large enough to significantly affect public health, as they would be below the public health-based *CEQR Technical Manual* noise threshold of 85 dBA. The Amended RWCDS is not anticipated to cause excessively high chronic noise exposure and, therefore, is not expected to result in a significant adverse public health impact related to noise. Consequently, the A-Application would not result in significant adverse public health impacts.

NEIGHBORHOOD CHARACTER

The DEIS concluded that the Proposed Actions would not result in any significant adverse impacts to neighborhood character. The following considers whether the incremental development associated with the Amended RWCDS would have the potential to modify the DEIS finding. The analysis finds that the Amended RWCDS would not result in any significant adverse impacts to neighborhood character.

The Downtown Far Rockaway study area can be characterized as a ‘village,’ with a commercial downtown providing the local community with commercial and institutional services typical of a village center, with surrounding residential use at varying densities. With the Amended With-Action

Condition, as with the Proposed Actions, the Downtown Far Rockaway area would continue to be defined as a commercial, ‘village’ like, downtown. The text amendments associated with the A-Application would alter the massing of buildings on the Proposed DFRURA. This would further improve the proposed development’s context with the surrounding area by altering the street wall location and recess, altering the minimum and maximum base heights of proposed buildings, and by limiting tall towers to the central portion of Subdistrict A. The concentration of taller buildings on Subdistrict A under the Amended RWCDS would improve overall building context with the surrounding neighborhood, much of which is built to a smaller scale. As such, the Actions proposed in the A-Application would enhance the street grid and built environment of Downtown Far Rockaway, and contribute to the Neighborhood Character of the area. Furthermore, the altered massing of buildings on the Proposed DFRURA site would not alter the conclusion of the shadow assessment of the DEIS. The zoning map amendments associated with the A-Application would extend the proposed Rezoning Area boundary to include two additional Projected Development Sites—Sites 18 and 19. No sunlight-sensitive resources are located within the longest shadow study areas of either Projected Development Sites 18 or 19, and there would be no resulting neighborhood character impacts.

The zoning map amendments associated with the A-Application also would extend a C2-4 commercial overlay along Mott Avenue, enabling a ground-floor retail use at Projected Development Site (as compared to a ground floor community facility use assumed in the DEIS). As Projected Development Site 9 is contiguous with other retail uses along Mott Avenue, this change in ground floor use serves to extend the retail corridor. Therefore, the change in use is consistent with existing land uses in the Project Area and does not alter neighborhood character. Projected Development Site 18 would introduce an indoor playroom and recreation/garden space for residents’ use; and a publicly accessible multipurpose room for community use. Projected Development Site 19 would be redeveloped with one building including residential use above a laundromat and larger ground floor supermarket, offering an improved amenity to the neighborhood. Commercial, residential, and community facility amenities are much needed within the neighborhood and would be welcomed by the community. As such, the Actions proposed in the A-Application would transform underutilized land on Projected Sites 18 and 19. Under the Amended With-Action condition, the proposed commercial, residential, and community facility amenities would be consistent with, and would complement, the existing land uses within the neighborhood.

There are no known architectural resources on Projected Development Sites 18 and 19. As identified in the DEIS, there are several historic resources within the neighborhood, however the Actions proposed in the A-Application would not alter the relationship between these historic resources and the streetscape. Additionally, no incompatible visual, audible, or atmospheric elements would be introduced under the Amended With-Action condition to interfere with the enjoyment of existing historic resources.

Similar to the RWCDS analyzed in the DEIS, the Amended RWCDS would result in increased transportation activities and significant adverse transportation impacts. The incremental development associated with the Amended RWCDS would not materially affect these identified impacts; traffic, transit, and pedestrian conditions would be similar to those seen in other urban neighborhoods and would not result in density of activity or service conditions that would be out of character with a typical downtown core.

Overall, similar to the Proposed Actions analyzed in the DEIS, the Actions proposed in the A-Application would facilitate the introduction of new mixed-income housing, retail, community facilities and open space that would enliven the streetscape and that would provide much-needed

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amenities to enhance the “village character” of the neighborhood. Under the Amended With-Action Condition, the effects to neighborhood character would be noticeable, in many respects positive, and not significantly adverse.

Conceptual Analysis: Authorization to Modify Bulk Regulations

As described above, the bulk modifications would not result in new significant adverse shadow or urban design impacts, and the modifications would not have the potential to affect other defining features of the neighborhood (e.g., changes to land use, open space, or traffic). The purpose of the zoning text amendment to authorize bulk modifications is to allow for better site planning that would benefit both the residents and occupants of the buildings as well as the surrounding neighborhood. The proposed zoning text amendment specifically states that in order to grant Authorization, CPC findings would require that the bulk modifications result in a superior site plan, in which the buildings subject to the authorization and any associated open areas relate harmoniously to one another and with adjacent buildings and open areas. In addition, as part of its authorization CPC could prescribe appropriate conditions and safeguards to minimize adverse effects on the character of the surrounding area. Therefore, authorization to modify bulk regulations would not alter the findings in the DEIS that the Proposed Actions would not result in significant adverse impacts to neighborhood character.

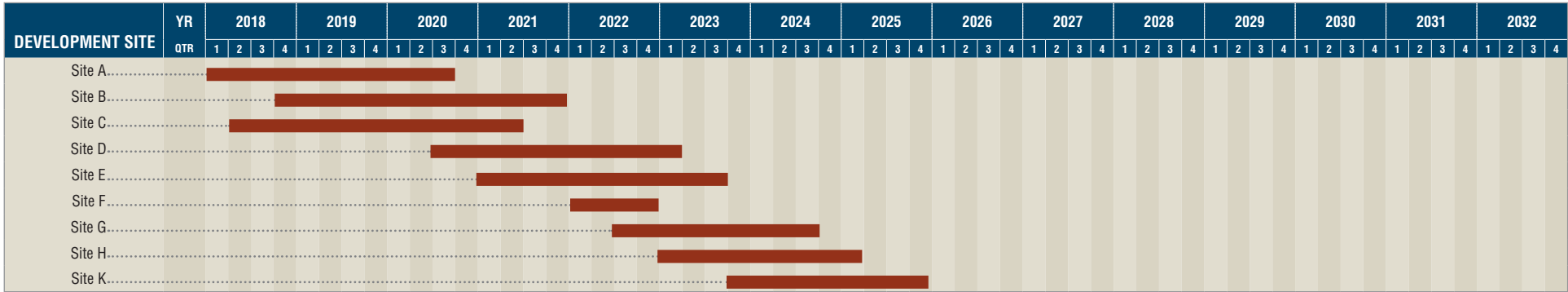
CONSTRUCTION

The analyses presented in the DEIS conservatively account for overlapping construction activities for development sites in proximity to one another to capture the cumulative nature of construction impacts. With the proposed extension of the Rezoning Area, two new Projected Development Sites (Projected Development Site 18 and 19) would be constructed. **Figure 14** and **Tables 20 and 21** present a conceptual schedule of construction under the Amended With-Action Condition, accounting for the shift in construction timing for Projected Development Site 9 and the addition of Projected Development Sites 18 and 19. Based on the construction durations for Projected Development Sites of similar development size, the construction duration at Projected Development Site 18 is estimated to be 21 months, while the construction at Projected Development Site 19 is estimated to be 18 months in duration.

Construction Air Quality

The DEIS construction air quality analysis included a detailed quantified modeling study of the most intensive construction periods and concluded that construction under the Proposed Actions would not result in significant adverse air quality impacts. Similarly, construction activities associated with the Amended RWCDS that includes the two additional Projected Development Sites are not expected to result in any significant adverse air quality impacts. Projected Development Sites 18 and 19 are anticipated to take approximately 21 and 18 months to construct, respectively, and are considered short-term (i.e., less than two years) in accordance with the *CEQR Technical Manual*. As described in the *CEQR Technical Manual*, any impacts from such short-term construction generally do not require detailed assessment. Although Projected Development Sites 18 and 19 are in proximity of other Projected Development Sites within the Rezoning Area, based on their development sizes, projected construction timelines, and anticipated construction activities, the addition of these two Projected Development Sites are not expected to result in more intense construction peak periods than the ones analyzed in the DEIS. Construction of Projected Development Sites 18 and 19 is required to follow all applicable regulations, laws, and codes, including the *New York City Air Pollution Control Code* regulations

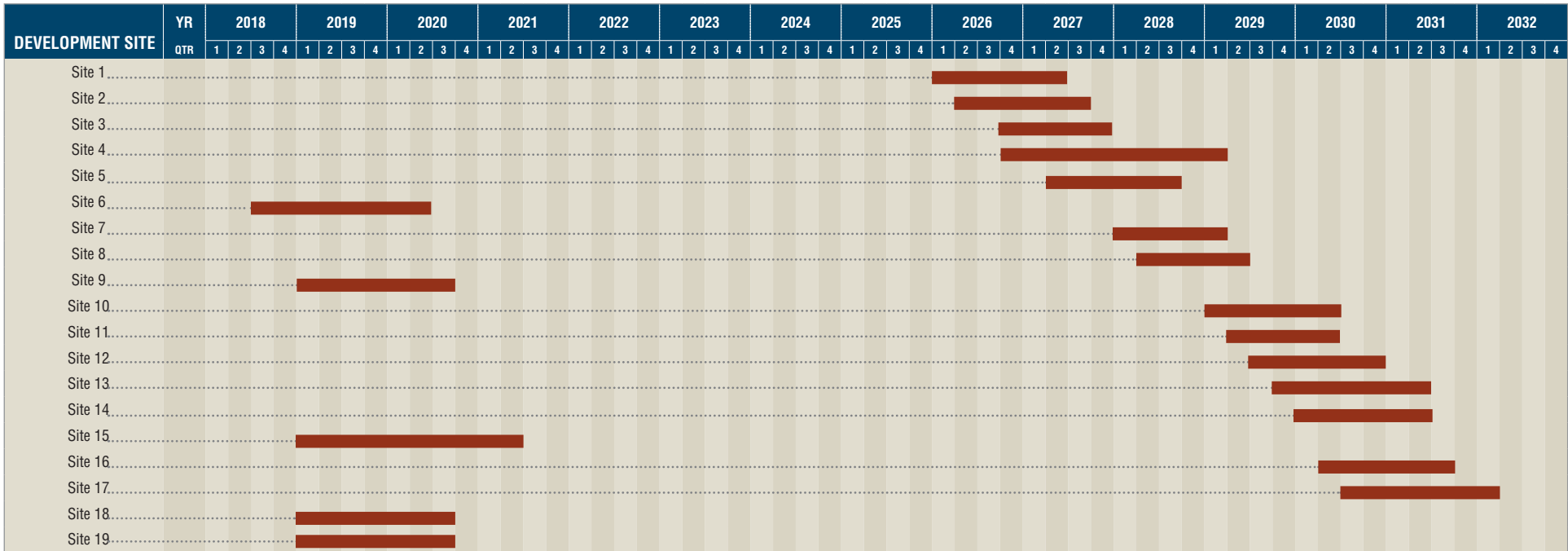
Downtown Far Rockaway Urban Renewal Area and Disposition Sites



Source: Hunter Roberts Construction Group

Note: The construction schedule for the DSNY Disposition Site was not estimated. Due to the small size to be developed with the Proposed Actions (8,000 gsf) and relative distance between the site and the major components of the Proposed DFRURA (approximately 55 feet north of the Proposed DFRURA and 690 feet from Sites B and C), construction activity would have an insignificant effect to the construction analysis.

Projected Development Sites



Source: Hunter Roberts Construction Group

regarding construction-related dust emissions, and to *New York City Administrative Code* limitations on construction-vehicle idling time.

Table 20
Conceptual Construction Schedule
Downtown Far Rockaway Urban Renewal Area and Disposition Sites
Amended With-Action Condition

Development Site	Start Month	Finish Month	Approximate duration (months)
Site A (MTA/DOT Disposition Site)	2018 (Q1)	2020 (Q3)	33
Site B	2018 (Q4)	2021 (Q4)	39
Site C	2018 (Q2)	2021 (Q2)	39
Site D	2020 (Q3)	2023 (Q1)	33
Site E	2021 (Q1)	2023 (Q3)	33
Site F	2022 (Q1)	2024 (Q1)	27
Site G	2022 (Q3)	2024 (Q3)	27
Site H	2023 (Q1)	2025 (Q1)	27
Site K	2023 (Q4)	2025 (Q4)	27

Note: The construction schedule for the DSNY Disposition Site was not estimated. Due to the small size to be developed with the Proposed Actions (8,000 gsf) and relative distance between the site and the major components of the Proposed DFRURA (approximately 55 feet north of the Proposed DFRURA and 690 feet from Sites B and C), construction activity would have an insignificant effect to the construction analysis..

Source: Hunter Roberts Construction Group

Table 21
Conceptual Construction Schedule
Projected Development Sites
Amended With-Action Condition

Projected Development Site	Start Month	Finish Month	Approximate duration (months)
Site 1	2026 (Q1)	2027 (Q2)	18
Site 2	2026 (Q2)	2027 (Q3)	18
Site 3	2026 (Q3)	2027 (Q4)	18
Site 4	2026 (Q4)	2029 (Q1)	30
Site 5	2027 (Q2)	2028 (Q3)	18
Site 6	2018 (Q3)	2020 (Q2)	24
Site 7	2028 (Q1)	2029 (Q1)	15
Site 8	2028 (Q2)	2029 (Q2)	15
Site 9	2019 (Q1)	2020 (Q3)	21
Site 10	2029 (Q1)	2030 (Q2)	18
Site 11	2029 (Q2)	2030 (Q2)	15
Site 12	2029 (Q3)	2030 (Q4)	18
Site 13	2029 (Q4)	2031 (Q2)	21
Site 14	2030 (Q1)	2031 (Q2)	18
Site 15	2019 (Q1)	2021 (Q2)	30
Site 16	2030 (Q2)	2031 (Q3)	18
Site 17	2030 (Q3)	2032 (Q1)	21
Site 18	2019 (Q1)	2020 (Q3)	21
Site 19	2019 (Q1)	2020 (Q3)	18

Source: Hunter Roberts Construction Group and AKRF, Inc.

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Construction Noise

The Amended RWCDS would not result in appreciable changes to the construction logistics, schedule, or equipment list for construction of buildings B, C, and D in the Proposed DFRURA, or for Projected Development Site 2. The DEIS included a detailed quantitative analysis of noise generated by construction of these buildings. The results of this detailed quantitative analysis would remain valid for construction of these buildings with the Amended RWCDS. However, noise resulting from construction of the additional Projected Development Sites (i.e., Projected Development Sites 18 and 19) introduced in the Amended RWCDS was analyzed according to the methodology used in the DEIS for Projected Development Sites. The DEIS analysis of Projected Development Sites was based on worst-case construction equipment and schedule assumptions from all Projected Development Sites, and was applied to Projected Development Site 2, which has many noise receptors immediately adjacent and nearby.¹²

As described in the DEIS construction noise analysis, the detailed analysis of noise associated with construction of Projected Development Site 2 found the potential for significant adverse noise impacts at receptors immediately adjacent to or on the same block as the construction site. Receptors in these areas were predicted to experience construction noise levels up to the mid 80s dBA, and noise level increments up to approximately 23 dBA. The highest levels of construction noise were predicted to occur as a result of pile driving activities and excavation activities, which would occur for approximately 6 months at each Projected Development Site (including Sites 18 and 19), although construction noise at certain locations within the Project Area was determined to have the potential to exceed CEQR impact thresholds for a duration of approximately 3 years.

Construction at Projected Development Sites 18 and 19 would employ the same noise control measures specified for the other Projected Development Sites in the DEIS (e.g., noise emission limits on specific pieces of equipment, site-perimeter noise barriers). The type and quantity of construction equipment and the construction schedule specific to Projected Development Sites 18 and 19 would be less than the worst-case construction schedule and equipment assumptions applied to Projected Development Sites in the DEIS and used for this analysis. Nevertheless, to provide for a more conservative analysis the magnitude and duration of noise effects associated with these worst-case conditions were applied to Projected Development Sites 18 and 19. Consequently, receptors immediately adjacent to or on the same block adjacent to Projected Development Sites 18 and 19 were determined to have the potential to experience comparable significant adverse noise impacts.

Based on the above, **Table 22** lists the noise receptor locations determined to have the potential to experience significant adverse construction noise impacts resulting from construction associated with Projected Development Sites 18 and 19 that were not already identified in the DEIS as having the potential to experience significant adverse construction noise impacts resulting from construction. These locations are a mix of residential, healthcare, and community facility receptors. The predicted noise level increases at these locations would be noticeable and would result in significant adverse impacts throughout the excavation and foundation construction of Projected Development Sites 18 and 19. Each location is adjacent to either Projected Development Site 18 or 19.

¹² While the projected development program for Projected Development Site 2 is not consistent with worst-case construction equipment and schedule assumptions, the analysis serves as a conservative estimate of worst-case construction conditions at the Projected Development Sites throughout the rezoning area. This will be clarified as part of the FEIS.

Table 22

Projected Development Sites 18 and 19: Significant Adverse Construction Noise Impact Locations

Location	Land Use
13-16 Central Avenue	Community Facility ⁽¹⁾
13-28 Central Avenue	Community Facility ⁽²⁾
13-37 Augustina Avenue	Residential
14-09 Gateway Boulevard	Residential
14-15 Mott Avenue	Residential
13-34 Caffrey Avenue	Residential
Notes: ⁽¹⁾ Arverne Church of God ⁽²⁾ Arverne Church of God Christian Academy	

At these receptors, for an approximately 3-month period during the use of impact pile drivers for building foundation construction and excavation at Sites 18 and 19, construction noise levels would be at their highest and noise level increases would be readily noticeable and likely intrusive. In addition, noise levels in the low 80s are relatively high for this area. Standard building façade construction along with an alternate means of ventilation allowing for the maintenance of a closed-window condition would be expected to provide approximately 25 dBA window/wall attenuation. With such measures, interior noise levels at these receptors are predicted to be in the low 40s to high 50s dBA, which is up to approximately 15 dBA higher than the 45 dBA threshold recommended for residential or community facility use according to CEQR noise exposure guidelines.

Since pile driving and the use of excavators have a limited duration, and each receptor is adjacent to only one or two of the Projected Development Sites, the maximum noise levels predicted by the construction noise analysis would not persist throughout the construction period. Nevertheless, construction noise levels occurring during activities other than pile driving or excavation operations may still result in exceedances of CEQR impact criteria at some times during the approximately 3 years of construction at an adjacent projected development site, but would be substantially lower than the maximum levels during pile driving.

As described above, predicted construction noise levels at these receptors were predicted to be in the low 60s to mid 80s dBA with noise level increases up to approximately 23 dBA. Interior noise levels at these receptors are predicted to be up to approximately 15 dBA above the acceptable range according to CEQR noise exposure criteria during the most noise-intensive phases of construction. Based on these factors, construction noise associated with Projected Development Sites 18 and 19 is predicted to result in a temporary significant adverse impact at these receptors.

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CONCLUSION

As described above, the proposed modifications would not result in any new or different significant adverse environmental impacts that were not previously identified in the DEIS.



Hilary Semel
Assistant to the Mayor

April 26, 2017
Date

Appendix A

Special Downtown Far Rockaway District Revised Text

SPECIAL DOWNTOWN FAR ROCKAWAY DISTRICT

Matter underlined is new, to be added;
Matter ~~struck-out~~ is to be deleted;
Matter within # # is defined in Section 12-10;
*** indicates where unchanged text appears in the Zoning Resolution

Article I: GENERAL PROVISIONS

Chapter 1 - Title, Establishment of Controls and Interpretation of Regulations

* * *
11-122
Districts established

* * *
Special Purpose Districts
* * *

Establishment of the Special Downtown Brooklyn District

In order to carry out the special purposes of this Resolution as set forth in Article X, Chapter 1, the #Special Downtown Brooklyn District# is hereby established.

Establishment of the Special Downtown Far Rockaway District

In order to carry out the special purposes of this Resolution as set forth in Article XIII, Chapter 6, the #Special Downtown Far Rockaway District# is hereby established.

Establishment of the Special Downtown Jamaica District

* * *
Chapter 2 – Construction of Language and Definitions

* * *
12-10
DEFINITIONS

* * *
Special Downtown Brooklyn District

The “Special Downtown Brooklyn District” is a Special Purpose District designated by the letters “DB” in which special regulations set forth in Article X, Chapter 1, apply.

Special Downtown Far Rockaway District

The “Special Downtown Far Rockaway District” is a Special Purpose District designated by the letters “DFR” in which special regulations set forth in Article XIII, Chapter 6, apply.

Special Downtown Jamaica District

* * *

Chapter 4 – Sidewalk Café Regulations

* * *

**14-44
Special Zoning Districts Where Certain Sidewalk Cafes Are Permitted**

#Enclosed# or #unenclosed sidewalk cafes# shall be permitted, as indicated, in the following special zoning districts, where allowed by the underlying zoning. #Small sidewalk cafes#, however, may be located on #streets# or portions of #streets# within special zoning districts pursuant to the provisions of Section 14-43 (Locations Where Only Small Sidewalk Cafes Are Permitted).

* * *

Queens	#Enclosed Sidewalk Cafe#	#Unenclosed Sidewalk Cafe#
<u>Downtown Far Rockaway District</u>	No	Yes
Downtown Jamaica District	No	Yes
Forest Hills District ¹	No	Yes
Long Island City Mixed Use District ²	No	Yes
Southern Hunters Point District	No	Yes
Willets Point District	No	Yes

¹ #Sidewalk cafes# are not allowed on Austin Street

² See Appendix A in Article XI, Chapter 7

* * *

Article II: RESIDENCE DISTRICT REGULATIONS

Chapter 3 – Residential Bulk Regulations in Residence Districts

* * *

**23-011
Quality Housing Program**

* * *

(c) In the districts indicated without a letter suffix, the optional Quality Housing #bulk# regulations permitted as an alternative pursuant to paragraph (b) of this Section, shall not apply to:

* * *

(2) Special Purpose Districts

However, such optional Quality Housing #bulk# regulations are permitted as an alternative to apply in the following Special Purpose Districts:

* * *

#Special Downtown Brooklyn District#;

#Special Downtown Far Rockaway District#;

#Special Downtown Jamaica District#;

* * *

**23-03
Street Tree Planting in Residence Districts**

R1 R2 R3 R4 R5 R6 R7 R8 R9 R10

In all districts, as indicated, the following shall provide #street# trees in accordance with Section 26-41 (Street Tree Planting):

- (a) #developments#, or #enlargements# that increase the #floor area# on a #zoning lot# by 20 percent or more. However, #street# trees shall not be required for #enlargements# of #single-# or #two-family residences#, except as provided in paragraphs (b) and (c) of this Section;
- (b) #enlargements# of #single-# or #two-family residences# by 20 percent or more within the following special purpose districts:

* * *

#Special Downtown Brooklyn District#;

#Special Downtown Far Rockaway District#;

#Special Downtown Jamaica District#;

* * *

**23-10
OPEN SPACE AND FLOOR AREA REGULATIONS**

R1 R2 R3 R4 R5 R6 R7 R8 R9 R10

* * *

**23-15
Open Space and Floor Area Regulations in R6 through R10 Districts**

R6 R7 R8 R9 R10

* * *

**23-153
For Quality Housing buildings**

R6 R7 R8 R9 R10

In the districts indicated, for #Quality Housing buildings#, the maximum #floor area ratio# and maximum #residential lot coverage# for #interior lots# or #through lots# shall be as set forth in the table in this Section. The maximum #residential lot coverage# for a #corner lot# shall be 100 percent.

The maximums for #zoning lots#, or portions thereof, located within 100 feet of a #wide street# in R6, R7 or R8 Districts without a letter suffix outside the #Manhattan Core#, shall be as designated by the same district with an asterisk. In an R6 District inside the #Manhattan Core# located within 100 feet of a #wide street#, the maximums shall be indicated by the same district with a double asterisk.

MAXIMUM LOT COVERAGE AND FLOOR AREA RATIO
FOR QUALITY HOUSING BUILDINGS

District	Maximum #Lot Coverage# for an #Interior Lot# or #Through Lot# (in percent)	Maximum #Floor Area Ratio#
R6	60	2.20
R6 ** ²	60	2.43
R6 * ^{1,3} R6A		
R7B	65	3.00
R6B	60	2.00
R7	65	3.44
R7 * ¹ R7A	65	4.00
	* * *	
R8 * ¹	70	7.20
	* * *	

- 1 for #zoning lots#, or portions thereof, located within 100 feet of a #wide street# in R6, R7 or R8 Districts without a letter suffix outside the #Manhattan Core#
- 2 for #zoning lots# in an R6 District inside the #Manhattan Core# located within 100 feet of a #wide street#
- 3 the maximum #lot coverage# for #zoning lots# in an R6 District utilizing the height and setback provisions of paragraph (a) of Section 23-952

**23-154
Inclusionary Housing**

* * *

- (d) Special #floor area# provisions for #zoning lots# in #Mandatory Inclusionary Housing areas#

For #zoning lots# in #Mandatory Inclusionary Housing areas#, the following provisions shall apply:

* * *

- (2) Maximum #floor area ratio#

The maximum #floor area ratio# for the applicable zoning district in #Inclusionary Housing designated areas# set forth in paragraph (b) of this Section shall apply to any #MIH development#. However, the maximum #floor area ratio# for any #MIH development# in an R6 District without a letter suffix shall be 3.6, in an R7-1 District shall be 4.6 and in an R7-3 or R7X District shall be 6.0; ~~the maximum #floor area ratio# shall be 6.0 for any #MIH development#.~~

* * *

**33-03
Street Tree Planting in Commercial Districts**

C1 C2 C3 C4 C5 C6 C7 C8

In all districts, as indicated, the following shall provide #street# trees in accordance with Section 26-41 (Street Tree Planting):

- (a) #developments#, or #enlargements# that increase the #floor area# on a #zoning lot# by 20 percent or more. However, #street# trees shall not be required for #enlargements# of #single-# or #two-family residences#, except as provided in paragraphs (b) and (c) of this Section;
- (b) #enlargements# of #single-# or #two-family residences# by 20 percent or more within the following special purpose districts:

* * *

#Special Downtown Brooklyn District#;

#Special Downtown Far Rockaway District#;

#Special Downtown Jamaica District#;

* * *

Article XIII - Special Purpose Districts

**Chapter 6
Special Downtown Far Rockaway District**

**136-00
GENERAL PURPOSES**

The “Special Downtown Far Rockaway District” established in this Resolution is designed to promote and protect the public health, safety and general welfare of the Downtown Far Rockaway community. These general goals include, among others, the following specific purposes:

- (a) strengthen the commercial core of Downtown Far Rockaway by improving the working and living environments;
- (b) support the development of vacant and underutilized parcels in Downtown Far Rockaway with a mix of residential, commercial and community facility uses;
- (c) encourage the design of new buildings to blend into the existing neighborhood fabric by providing a transition in height between the downtown commercial core and the lower-scale residential communities;

- (d) establish a center to the downtown with lively new gathering and civic spaces along Mott Avenue that complement and strengthen the existing neighborhood;
- (e) encourage the development of affordable housing;
- (f) expand the retail, entertainment and commercial character of areas around transit nodes to enhance the area's role as a local transportation hub;
- (g) integrate new roadways into an improved pedestrian and vehicular network with key north-south and east-west connections;
- (h) ensure the provision of adequate accessory parking that reflects both the automobile ownership patterns of the neighborhood and public transit access;
- (i) enhance the pedestrian environment by relieving sidewalk congestion and providing pedestrian amenities; and
- (j) promote the most desirable use of land and building development and thus conserve and enhance the value of land and buildings, and thereby protect the City's tax revenues.

136-01
General Provisions

The regulations of this Chapter shall apply within the #Special Downtown Far Rockaway District#. The regulations of all other chapters of this Resolution are applicable except as modified, supplemented or superseded by the provisions of this Chapter. In the event of a conflict between the provisions of this Chapter and other regulations of this Resolution, the provisions of this Chapter shall control.

136-02
Definitions

Definitions specifically applicable to this Chapter are set forth in this Section. The definitions of other defined terms are set forth in Section 12-10 (DEFINITIONS).

Open Space A

“Open Space A” shall be a publicly accessible open space designed and constructed pursuant to the provisions of Section 136-324 (Publicly accessible open space requirements) and located within the area designated as “Flexible Open Space A Location” on Map 7 (Mandatory Street Walls and Public Open Spaces) in the Appendix to this Chapter.

Open Space B

“Open Space B” shall be a publicly accessible open space designed and constructed pursuant to the provisions of Section 136-324 (Publicly accessible open space requirements) and located within the area designated as “Flexible Open Space B Location” on Map 7 (Mandatory Street Walls and Public Open Spaces) in the Appendix to this Chapter.

136-03
District Plan and Maps

The regulations of this Chapter implement the #Special Downtown Far Rockaway District# Plan.

The District Plan includes the following maps in the Appendix to this Chapter:

- Map 1 - Special Downtown Far Rockaway District and Subdistrict
- Map 2 - Commercial Core
- Map 3 - Ground Floor Use and Transparency Requirements

- Map 4 - Maximum Building Height
- Map 5 - Maximum Building Height Within Subdistrict A
- Map 6 - Publicly Accessible Private Streets
- Map 7 - Mandatory Street Walls and Public Open Spaces
- Map 8 - Sidewalk Widening

The maps are hereby incorporated and made part of this Resolution for the purpose of specifying locations where the special regulations and requirements set forth in this Chapter apply.

136-04
Subdistricts

In order to carry out the purposes and provisions of this Chapter, Subdistrict A is established. The location of the Subdistrict is shown on Map 1 in the Appendix to this Chapter.

136-05
Applicability of District Regulations

136-051
Applicability of the Quality Housing Program

R6 R7-1

In the districts indicated, and in C2 Commercial Districts mapped within such districts, any #building# containing #residences#, #long-term care facilities# or philanthropic or non-profit institutions with sleeping accommodations that is constructed in accordance with the #bulk# regulations of this Chapter shall be considered a #Quality Housing building#, and shall comply with the provisions of Article II, Chapter 8 (The Quality Housing Program) of this Resolution.

136-052
Applicability of the Mandatory Inclusionary Housing Program

R6 R7-1

In the districts indicated, and in C2 Commercial Districts mapped within such districts, the regulations for a #Mandatory Inclusionary Housing area# shall apply. The locations of such #Mandatory Inclusionary Housing areas# are shown on the maps in Appendix F of this Resolution.

136-06
Private Streets and Publicly Accessible Open Spaces

Except as otherwise provided herein, private streets that are provided in accordance with the provisions of this Chapter within the locations shown on Map 6 (Publicly Accessible Private Streets) in Appendix A of this Chapter, and publicly accessible open spaces that are provided in accordance with the provisions of this Chapter within the locations shown on Map 7 (Mandatory Street Walls and Public Open Spaces) in Appendix A of this Chapter shall be considered #streets# for the purposes of establishing the #use#, #bulk# and parking regulations of this Resolution. However, for the purposes of #floor area# regulations, such private streets and publicly accessible open spaces shall be considered part of a #zoning lot#. Furthermore, for the purpose of determining minimum and maximum base heights and minimum setback depth pursuant to Section 136-313 (Minimum and maximum base height), private streets and publicly accessible open spaces shall be distinguished from #streets#.

136-10
SPECIAL USE REGULATIONS

The #use# regulations of the underlying district shall apply except as modified in this Section, inclusive.

136-11

Location Within Buildings

Within locations shown on Map 2 (Commercial Core) in the Appendix to this Chapter, the provisions of Section 32-421 (Limitation on floors occupied by commercial uses) shall not apply. In lieu thereof, the provisions of Section 32-422 (Location of floors occupied by commercial uses) shall apply.

136-12

Use Groups 10A and 12 in C2 Districts

Within locations shown on Map 2 (Commercial Core) in the Appendix to this Chapter, the provisions of Sections 32-19 (Use Group 10) and 32-21 (Use Group 12) shall be modified to allow Use Groups 10A and 12 in C2 Districts.

136-13

Ground Floor Use Regulations

The special ground floor #use# provisions of this Section shall apply to any #building#:

- (a) fronting on a designated #street#, as shown on Map 3 (Ground Floor Use and Transparency Requirements); or
- (b) located within 175 feet of Mott Avenue and fronting on #Open Space A#.

#Uses# within #stories# on the ground floor or with a floor level within five feet of the level of the adjoining sidewalk, shall be limited to non-#residential uses#. A #building's street# frontage shall be allocated exclusively to such #uses#, except for Type 1 lobby space, entrances and exits to #accessory# off-street parking facilities, and entryways or entrances to subway stations in accordance with Section 37-33 (Maximum Width of Certain Uses). Such non-#residential uses# shall comply with the minimum depth provisions of Section 37-32 (Ground Floor Depth Requirements for Certain Uses).

136-14

Transparency and Parking Wrap Requirements

The provisions of this Section shall apply to #buildings developed# or #enlarged# after [date of adoption], where the ground floor of such #development# or #enlarged# portion of the #building# fronts upon designated #streets# as shown on Map 3 (Ground Floor Use and Transparency Requirements) in the Appendix to this Chapter. These provisions shall also apply to the frontage of #buildings# located along #Open Space A#. The ground floor #street wall# of such #building# or portion thereof shall be glazed in accordance with Section 37-34 (Minimum Transparency Requirements).

The provisions of Section 37-35 (Parking Wrap and Screening Requirements) shall apply along designated #streets# as shown on Map 3 and along #Open Space A#. In addition, the screening requirements of paragraph (b) of Section 37-35 shall apply along intersecting #streets# within 50 feet of designated #streets#, and along intersecting #streets# or private streets within 50 feet of #Open Space A#.

136-15

Special Use Regulations Within Subdistrict A

The following additional special #use# provisions of this Section, inclusive, shall apply within Subdistrict A, as shown on Map 1 (Special Downtown Far Rockaway District and Subdistrict) in the Appendix to this Chapter.

136-151

Modification of Supplemental Use Provisions

For #mixed buildings#, the underlying provisions of Section 32-421 (Limitation on floors occupied by non-residential uses) shall not apply. In lieu thereof, Use Groups 6, 7, 8, 9 or 14, other than offices listed in Use Group 6B, shall not be located above the level of the second #story# ceiling. Offices shall be permitted above the level of the second #story#, provided that where any floor space allocated to such offices is located on the same #story# as a #dwelling unit#, no access exists between such #uses#, and further provided that no floor space allocated to such offices is located directly over #dwelling units#.

136-152

Location of entrances

(a) Non-#residential# entrances

Within Subdistrict A, on designated #streets#, as shown on Map 3 (Ground Floor Use and Transparency Requirements) in the Appendix to this Chapter, the requirements of this paragraph (a) shall apply to any #building or other structure# fronting on such #streets#. These provisions shall also apply to the frontage of #buildings# along #Open Space A#. Access to each ground floor #commercial# or #community facility# establishment shall be provided directly from a #street# or from #Open Space A#.

(b) #Residential# entrances

Eighty percent of all ground floor #dwelling units# with frontage only on Redfern Avenue shall have a #primary entrance# directly accessible from Redfern Avenue.

136-20

SPECIAL BULK REGULATIONS

The #bulk# regulations of the underlying district shall apply except as modified in this Section, inclusive.

136-21

Lot Coverage

The #residential# portion of a #building# shall comply with the maximum #lot coverage# provisions of the underlying district applicable to #Quality Housing buildings#.

136-22

Height and Setback Regulations

For #residential buildings#, #mixed buildings# and #commercial buildings#, the height and setback regulations of the underlying district shall be modified by the regulations of this Section, inclusive. The provisions of Section 23-952 (Height and setback in Mandatory Inclusionary Housing areas) and Section 23-664 (Modified height and setback regulations for certain Inclusionary Housing buildings or affordable independent residences for seniors) shall not apply within the #Special Downtown Far Rockaway District#.

All heights shall be measured from the #base plane#.

136-221

Street wall location

In C2 Districts, the #street wall# location regulations of the underlying district shall apply except as modified in this Section.

- (a) In C2 Districts mapped within R6 and R7-1 Districts, at least 70 percent of the #aggregate width of street walls# shall be located within eight feet of the #street line# and shall extend to at least the minimum base height specified in Section 136-222 (Minimum and maximum base height), or the height of the #building#, whichever is less. Up to 30 percent of the #aggregate width of street walls# may be recessed beyond eight feet of the #street line#, provided that any such recesses deeper than ten feet along a #wide street# or 15 feet along a #narrow street# are located within an #outer court#. For #zoning lots# bounded by more than one #street line#, these #street wall# location requirements shall be mandatory on only one #street line#.
- (b) In C2 Districts mapped within R5 Districts, at least 70 percent of the #aggregate width of street walls# shall be located within eight feet of the #street line# and shall extend to a height of 30 feet, or the height of the #building#, whichever is less. Up to 30 percent of the #aggregate width of street walls# may be recessed beyond eight feet of the #street line#, provided that any such recesses deeper than ten feet along a #wide street# or 15 feet along a #narrow street# are located within an #outer court#. For #zoning lots# bounded by more than one #street line#, these #street wall# location requirements shall be mandatory on only one #street line#.
- (c) Below a height of 15 feet or the height of the second #story# floor, whichever is lower, no recesses shall be permitted within 30 feet of the intersection of two #street lines# except recesses that do not exceed a depth of 12 inches.

The underlying allowances for #street wall# articulation, set forth in paragraph (e) of Section 35-651 (Street wall location) shall be permitted to project or recess beyond the #street wall# locations established in paragraphs (a) and (b) of this Section.

Existing #buildings# may be vertically #enlarged# by up to one #story# or 15 feet, without regard to the #street wall# location requirements of this Section.

136-222

Minimum and maximum base height

R6 R7-1

In the districts indicated, and in C2 Commercial Districts mapped within such districts, the minimum and maximum heights before setback of a #street wall# required pursuant to Section 136-221 (Street wall location), shall be as set forth in the following table:

<u>District</u>	<u>Minimum Base Height (feet)</u>	<u>Maximum Base Height (feet)</u>
<u>R6</u>	<u>30</u>	<u>55</u>
<u>R7-1</u>	<u>40</u>	<u>55</u>

At a height not lower than the minimum base height nor higher than the maximum base height specified for the applicable district in this Section, a setback with a depth of at least ten feet shall be provided from any #street wall# fronting on a #wide street#, and a setback with a depth of at least 15 feet shall be provided from any #street wall# fronting on a #narrow street#. The underlying provisions of paragraphs (c)(2) through (c)(4) of Section 23-662 (Maximum height of buildings and setback regulations) shall apply to such setbacks.

136-223

Maximum building height

R6 R7-1

In the districts indicated, and in C2 Commercial Districts mapped within such districts, the height of a #building or other structure# shall not exceed the maximum height or the maximum number of #stories#, whichever is less, as shown for such location on Map 4 (Maximum Building Height) in the Appendix to this Chapter.

136-30

SPECIAL REGULATIONS WITHIN SUBDISTRICT A

The regulations of this Section, inclusive, shall apply within the area labeled “Subdistrict A”, as shown on Map 1 (Special Downtown Far Rockaway District and Subdistrict) in the Appendix to this Chapter. The regulations of the #Special Downtown Far Rockaway District# shall apply, except as modified by the regulations of this Section, inclusive.

136-31

Special Height and Setback Regulations Within Subdistrict A

136-311

Street wall location

The provisions of Section 136-221 (Street wall location) shall apply within Subdistrict A, except as provided in this Section.

(a) For portions of #buildings# or #building segments# with frontage on Redfern Avenue located between the prolongation of the northerly #street line# of Dix Avenue and a line 150 feet south of and parallel to Nameoke Street, the street wall location rules of Section 136-221 shall not apply. In lieu thereof, paragraph (b) of Section 23-661 (Street wall location) shall apply.

(b) For “Street Wall A” and “Street Wall B”, as shown on Map 7 (Mandatory Street Walls and Public Open Spaces) in the Appendix to this Chapter, the provisions of Section 136-231 (Street wall location) shall not apply. In lieu thereof, the provisions of this Section shall apply.

(1) “Street Wall A”

#Buildings# on the west side of #Open Space A# shall have a #street wall# located along the required sidewalk widening on Mott Avenue, shown as a line designated “A1” on Map 7, except that #street wall# articulation set forth in paragraph (e) of Section 35-651 (Street wall location) shall be permitted. Beyond 112 feet of Redfern Avenue, the #street wall# shall be located no closer to Central Avenue than the line designated “A2” as shown on Map 7.

(2) “Street Wall B”

#Street walls# fronting #Open Space A# shall be located no closer to Redfern Avenue than as shown as a line designated “B1” on Map 7. The #street walls# of #buildings# on the east side of #Open Space A# with frontage on Mott Avenue shall be located no closer to Mott Avenue than as shown as lines designated “B2” and “B3” on Map 7 for. Portions of #street walls# with frontage on Mott Avenue, located so that a line drawn perpendicular to the line designated “B3” intersects such #street walls#, shall be located no further than 30 feet from “B3.” #Street walls# fronting Central Avenue shall be located no closer to Central Avenue than as shown for the line designated “B4” on Map 7, and shall be located no further than 30 feet from “B4.”

(c) For #blocks# with a dimension of less than 100 feet between #streets# or private streets that are parallel or do not intersect, the provisions of Section 136-221 shall be modified to require a minimum of 40 percent of the #aggregate width of street walls# to be located within eight feet of the #street line# and to extend to at least the minimum base height specified in Section 136-222 (Minimum and maximum base height), or the height of the #building#, whichever is less.

All #street walls# governed by this Section shall extend to the minimum base height specified in Section 136-313 (Minimum and maximum base height), or the height of the #building#, whichever is less.

136-312
Street wall recesses

For each #building# within Subdistrict A, where the #aggregate width of street walls# is greater than 90 feet, a minimum of 20 percent of the surface area of #street walls# below the maximum base height and above the level of the first #story# shall be recessed beyond three feet of the #street line#. Portions of #street lines# with no #street walls# may be counted towards the recess requirements of this Section. No portion of such minimum recessed area shall be located within 30 feet of the intersection of two #street lines#. However, such minimum recessed area shall be permitted within 30 feet of Redfern Avenue, except at the intersection of Redfern Avenue and Mott Avenue.

136-313
Minimum and maximum base height

Within Subdistrict A, the provisions of Section 136-222 (Minimum and maximum base height) shall not apply. In lieu thereof, for #residential buildings#, #mixed buildings# and #commercial buildings#, the provisions of this Section shall apply. The #street wall# height and setback regulations of the underlying district shall apply except as modified in this Section.

(a) The minimum and maximum heights before setback of a #street wall# required pursuant to Section 136-221 (Street wall location), shall be as set forth in the following table:

<u>Condition</u>	<u>Minimum Base Height (feet)</u>	<u>Maximum Base Height (feet)</u>	<u>Minimum Setback Depth (feet)</u>
<u>Fronting on Redfern Avenue and greater than 75 feet from an intersecting #street#</u>	<u>30</u>	<u>45</u>	<u>10</u>
<u>Fronting on, or within 100 feet of, a #street#, other than a private street or publicly accessible open space</u>	<u>40*</u>	<u>65</u>	<u>10</u>
<u>Fronting on a private street or a publicly accessible open space and beyond 100 feet of a #street# that is not a private street or publicly accessible open space</u>	<u>40</u>	<u>85</u>	<u>7</u>

* Within 300 feet of Mott Avenue, the minimum base height shall be 20 feet.

(b) Dormers

The provisions of paragraph (c) of Section 23-621 (Permitted obstructions in certain districts) shall be modified to allow dormers as a permitted obstruction within the required front setback distance above a maximum base height, as follows:

- (1) Within 75 feet of intersecting #streets#, dormers shall be permitted without limitation on width.
- (2) Where dormers are provided pursuant to paragraph (b)(1) of this Section, and such dormers exceed the maximum width permitted pursuant to paragraph (c) of Section 23-621, for any portion of a #building# with an #aggregate width of street walls# greater than 75 feet, a setback shall be provided above the maximum base height between such dormer and any other dormer for a width of at least 20 feet, or the remaining width of such #street wall#, as applicable.
- (3) Beyond 75 feet of intersecting #streets#, the provisions of paragraph (c) of Section 23-621 shall apply. The width of any dormers provided pursuant to the provisions of paragraph (b)(1) of this Section shall be included in the aggregate width of all dormers.

However, the provisions of this paragraph (b) shall not apply to portions of #buildings# with frontage on Redfern Avenue, except that these provisions shall apply to portions of #buildings# with frontage on both Redfern Avenue and Mott Avenue.

136-314

Maximum building height

The height of a #building or other structure# shall not exceed the maximum building height or the maximum number of #stories#, whichever is less, as shown on Map 5 (Maximum Building Height Within Subdistrict A) in the Appendix to this Chapter. However, within 75 feet of the intersection of the southerly cross street with Redfern Avenue, and within 75 feet of the intersection of the northerly cross street with Redfern Avenue, #buildings or other structures# shall not exceed maximum height of six #stories# or 65 feet, whichever is less.

136-315

Maximum building height and horizontal dimension for tall buildings

Within the area labeled “Tower Location Area” on Map 5 (Maximum Building Height Within Subdistrict A) in the Appendix to this Chapter, the height of a #building# may exceed the height limits specified in Section 136-314 (Maximum building height) only as set forth in this Section. Any portion of a #building# above a height of 125 feet shall hereinafter be referred to as a “tower.”

- (a) Towers shall be located within portions of #zoning lots# bounded by intersecting #street lines# and lines parallel to and 200 feet from each intersecting #street line#.
- (b) Towers shall be separated from one another by a minimum distance of 60 feet, measured in all horizontal directions.
- (c) The outermost walls of each #story# located entirely above a height of 125 feet shall be inscribed within a rectangle. The maximum length of two sides of such rectangle shall be 170 feet. The maximum length of the other two sides of such rectangle shall be 100 feet. For the purposes of this Section, #abutting# portions of #buildings# above a height of 125 feet shall be considered a single tower.
- (d) To permit portions of a #building# to rise from grade to a tower portion without setback, the setback provisions of Section 136-313 (Minimum and maximum base height) shall not apply to any portion of a #building# located within 100 feet of intersecting #street lines#.
- (e) The maximum height of a tower shall be 155 feet or 15 #stories#, whichever is lower.
- (f) No more than two towers shall be permitted within Subdistrict A.

136-316

Maximum length of buildings

The outermost walls of each #story# located entirely above a height of nine #stories# or 95 feet, whichever is less, shall be inscribed within a rectangle. The maximum length of any side of such rectangle shall be 170 feet. For the purposes of this Section, #abutting buildings# on a single #zoning lot# shall be considered a single #building#.

136-32

Streets and public open spaces

136-321

Certification

The requirements of this Section shall apply to #zoning lots# containing #developments# or #enlargements# within the current or former Downtown Far Rockaway Urban Renewal Area. No building permit shall be issued for any #development# or #enlargement# until the Chairperson of the City Planning Commission certifies to the Department of Buildings that such #development# or #enlargement# complies with the provisions of this Section.

The Chairperson shall certify that:

- (a) the Department of City Planning has received a letter from the Commissioner of Transportation confirming that the design of any proposed private streets and sidewalks adjacent to the proposed #development# or #enlargement# complies with Department of Transportation standards, or, where the design varies from such standards, the design is acceptable to the Commissioner;
- (b) all publicly accessible open spaces adjacent to the proposed #development# or #enlargement# comply with the provisions of Section 136-324 (Publicly accessible open space requirements);
- (c) the location of private streets adjacent to the proposed #development# or #enlargement# complies with the provisions of Section 136-323 (Private streets); and
- (d) for any portion of Subdistrict A outside the area of the proposed #development# or #enlargement# for which a certification pursuant to this Section has not been obtained, the applicant has submitted sufficient documentation showing that the #development# or #enlargement# that is the subject of this certification, and any associated private streets and publicly accessible open spaces required to be constructed in conjunction with such #development# or #enlargement#, shall not preclude such undeveloped portions of Subdistrict A from complying with the provisions of Sections 136-323 and 136-324 under future certifications pursuant to this Section.

All required private streets and publicly accessible open spaces, once certified in accordance with the provisions of this Section, shall be duly recorded in the form of a signed declaration of restrictions, including provisions for the maintenance and operation of such private streets and publicly accessible open spaces, indexed against the property, binding the owners, successors and assigns to provide and maintain such private streets and publicly accessible open spaces in accordance with the plans certified by the Chairperson. Such declaration, or any maintenance and operation agreement with the City or its designee executed in connection therewith, shall require that adequate security be provided to ensure that the private streets and public access areas are maintained in accordance with the declaration and any related maintenance and operation agreement and are closed only at authorized times. The filing of such declaration in the Borough Office of the Register of the City of New York shall be a precondition for the issuance of a building permit.

In addition, the private streets and publicly accessible open spaces integral to the #development#

or #enlargement# of a #building#, as indicated in the plans certified by the Chairperson, shall be recorded on the certificate of occupancy for such #building# by the Department of Buildings. The recording information of the declaration of restrictions shall be included on the certificate of occupancy for any #building#, or portion thereof, issued after the recording date.

The property owner shall be responsible for the construction and maintenance of all required private streets and publicly accessible open spaces on the #zoning lot#. No temporary or final certificate of occupancy shall be issued for any #building# adjacent to such private street or publicly accessible open space until all required improvements are completed, except as otherwise provided in a phasing plan that has been incorporated in a signed and duly recorded declaration of restrictions.

136-322

Sidewalk widening

For #buildings developed# or #enlarged# after [date of adoption], where the #development# or horizontal #enlargement# fronts upon designated #streets# as shown on Map 8 (Sidewalk Widening) in the Appendix to this Chapter, the provisions of this Section shall apply.

A sidewalk widening is a continuous, paved open area along the #street line# of a #zoning lot#, located within the #zoning lot#. A sidewalk widening shall be provided along #streets# as shown on Map 8, to the extent necessary, so that a minimum sidewalk width of 13 feet or 18 feet, as applicable, is achieved, including portions within and beyond the #zoning lot#. Such depth shall be measured perpendicular to the #street line#. Sidewalk widenings shall be improved as sidewalks to Department of Transportation standards, at the same level as the adjoining public sidewalk and shall be directly accessible to the public at all times. No #enlargement# shall be permitted to decrease the depth of such sidewalk widening to less than such minimum required depth.

Lighting shall be provided with a minimum level of illumination of not less than two horizontal foot candles throughout the entire mandatory sidewalk widening. Lighting fixtures installed by the Department of Transportation within the #street# adjacent to such sidewalk widening shall be included in the calculation of the required level of illumination.

Where a continuous sidewalk widening is provided on the #zoning lot#, along the entire #block# frontage of a #street#, the boundary of the sidewalk widening within the #zoning lot# shall be considered to be the #street line# for the purposes of Sections 136-22 (Height and Setback Regulations) and 136-31 (Special Height and Setback Regulations Within Subdistrict A).

136-323

Private streets

In Subdistrict A, private streets shall be accessible to the public at all times, except when required to be closed for repairs, and for no more than one day each year in order to preserve the private ownership of such area. Private streets shall have a minimum width of 60 feet. Private streets shall be constructed to Department of Transportation standards for public #streets#, including lighting, signage, materials, crosswalks, curbs and curb cuts. Private streets shall include a paved road bed with a minimum width of 22 feet from curb to curb and sidewalks with a minimum clear path of 7 feet on each side along the entire length of the private street. Such private streets shall be located as shown on Map 6 (Publicly Accessible Private Streets) in the Appendix to this Chapter. One street tree shall be planted for every 25 feet of curb length of each private street. Fractions equal to or greater than one-half resulting from this calculation shall be considered to be one tree. Such trees shall be planted at approximately equal intervals along the entire length of the curb of the private street.

The private street network shall be established as follows.

- (a) A central street shall connect #Open Space A# with Nameoke Ave. as shown on Map 6 (Publicly Accessible Private Streets). However, if the centerline of the new street is not within five feet of the extended centerline of Brunswick Avenue, then the easterly curb of

the new street shall be greater than 50 feet from the extended line of the westerly curb of Brunswick Avenue.

- (b) A southerly cross street shall connect Redfern Avenue with the central street, intersecting Redfern Avenue within the 170-foot wide area shown on Map 6. However, if the centerline of the new street is not within five feet of the extended centerline of Dix Avenue, then the northerly curb of the new street shall be greater than 50 feet from the extended line of the southerly curb of Dix Avenue.
- (c) A northerly cross street shall connect Birdsall Avenue with Bayport Place, intersecting Redfern Avenue so that the centerline of the new street is within five feet of the extended centerline of Birdsall Avenue and within five feet of the centerline of Bayport Place.

136-324

Publicly accessible open space requirements

Publicly accessible open spaces shall be provided within the areas designated “Flexible Open Space A Location” and “Flexible Open Space B Location”, as applicable, as shown on Map 7 (Mandatory Street Walls and Public Open Spaces) in the Appendix to this Chapter. #Open Space A# shall contain a minimum of 23,000 square feet, and #Open Space B# shall contain a minimum of 7,000 square feet.

- (a) A portion of the required publicly accessible open space located within #Open Space A# shall have a minimum width of 80 feet within 55 feet of Mott Avenue. #Open Space A# shall extend from Mott Avenue to the nearest private street required pursuant to Section 136-323 (Private streets), and shall maintain a minimum width of 60 feet.
- (b) Publicly accessible open spaces shall comply with the provisions of Sections 37-725 (Steps), 37-726 (Permitted obstructions), 37-728 (Standards of accessibility for persons with disabilities), 37-73 (Kiosks and Open Air Cafes), 37-74 (Amenities) and 37-75 (Signs), except for the following modifications:
 - (1) Section 37-73 (Kiosks and Open Air Cafes) shall be modified as follows:
 - (i) Paragraph (a) of Section 37-73 shall be modified to permit a kiosk to occupy an area no greater than 400 square feet within #Open Space A#, provided that such kiosk has a maximum width, measured along the same axis as the minimum width of #Open Space A# pursuant to paragraph (a) of this Section, of 20 feet, and provided further that any canopies, awnings or other sun control devices extending from such kiosk shall be limited to a distance of five feet from such kiosk;
 - (ii) Paragraph (b) of Section 37-73 shall be modified to limit the aggregate area of open air cafes to no more than 40 percent of the publicly accessible open space, to allow open air cafes to occupy up to 50 percent of #street# frontage along Mott Avenue, and to eliminate the requirement that open air cafes be located along the edge of the publicly accessible open space; and
 - (iii) Paragraphs (c) and (d) of Section 37-73 shall not apply to the certification of open air cafes in the Special District, and the filing of plans for open air cafes in the Borough Office of the City Register shall not be required;
 - (2) Section 37-741 (Seating) shall be modified as follows:
 - (i) the requirement for a minimum of one linear foot of required seating for every two linear feet of #street# frontage within 15 feet of the #street line# shall not apply;

- (ii) the requirement of one linear foot of seating for each 30 square feet of #public plaza# area shall be modified to one linear foot of seating for each 60 square feet of publicly accessible open space; and
- (iii) seating for open air cafes may count toward the seating requirement, in the category of moveable seating, provided that 50 percent of the linear seating capacity is provided through other seating types;
- (3) For #Open Space A#, Section 37-742 (Planting and trees) shall be modified to require that at least 15 percent of the area of the publicly accessible open space shall be comprised of planting beds with a minimum dimension of two feet, exclusive of any bounding walls. For #Open Space B#, Section 37-742 (Planting and trees) shall be modified to eliminate the requirement for such planting beds;
- (4) Section 37-743 (Lighting) shall be modified to provide that for publicly accessible open spaces fronting on Mott Avenue, the lighting fixtures installed by the Department of Transportation within the #street# shall be included in the calculation of the required level of illumination;
- (5) Section 37-744 (Litter receptacles) shall be modified to require a minimum of one litter receptacle per 10,000 square feet of publicly accessible open space;
- (6) Entry plaques for publicly accessible open spaces shall be provided as described in paragraph (a) of Section 37-751 (Public space signage systems), except that the number of such plaques shall be provided so that one such plaque is located at each point of entry from a #street# to such publicly accessible open space. Plaques pursuant to paragraphs (b) and (c) of Section 37-751 shall not be required; and
- (7) Section 37-753 (Accessory signs) shall be modified as follows:
 - (i) paragraphs (a), (c) and (d) shall not apply;
 - (ii) paragraph (b) shall be modified to permit non-#illuminated# or #illuminated accessory signs#, and the permitted #surface area# of such #signs# shall be as permitted by the underlying district, as if the publicly accessible open space was a #street#; and
 - (iii) paragraph (e) shall be modified to permit any number of #accessory signs# within the publicly accessible open space, subject to the remaining provisions of such paragraph (e).

136-40

SPECIAL OFF-STREET PARKING REGULATIONS

136-41

Parking Regulations

The off-street parking regulations shall be modified, as follows:

- (a) The regulations of Section 25-027 (Applicability of regulations in Community District 14, Queens) shall not apply. In lieu thereof, the regulations of the applicable underlying district shall apply, as modified by the provisions of this Section.
- (b) In a C2 Commercial District mapped within an R7-1 District, the regulations of Section 25-251 (Income-restricted housing units) shall be modified to require an #accessory# off-street parking requirement of 25 percent per #income-restricted housing unit#.

- (c) For #commercial uses# in Parking Requirement Categories PRC-A, PRC-B, PRC-B1 and PRC-C, the provisions of Section 36-21 (General Provisions) shall be modified to require #accessory# off-street parking spaces at a rate of one parking space per 750 square feet of #floor area#.

For ambulatory diagnostic or treatment health care facilities listed in Use Group 4, the provisions of Sections 25-31 (General Provisions) and 36-21 shall be modified to require #accessory# off-street parking spaces at a rate of one parking space per 750 square feet of #floor area#.

- (d) Within Subdistrict A, parking spaces provided on private streets shall count towards the number of #accessory# off-street parking spaces required by the provisions of Sections 36-20 (REQUIRED ACCESSORY OFF-STREET PARKING SPACES FOR COMMERCIAL OR COMMUNITY FACILITY USES) and 36-30 (REQUIRED ACCESSORY OFF-STREET PARKING SPACES FOR RESIDENCES WHEN PERMITTED IN COMMERCIAL DISTRICTS). For such parking spaces located within private streets, the provisions of Section 28-40 (PARKING FOR QUALITY HOUSING) shall not apply.

136-50 **AUTHORIZATIONS**

136-51 **Authorization to Modify Standards for Publicly Accessible Open Spaces and Private Streets**

The City Planning Commission may authorize modification of the provisions of Sections 136-323 (Private streets) and 136-324 (Publicly accessible open space requirements), provided that the Commission shall find that:

- (a) the usefulness and attractiveness of the publicly accessible open space will be improved by the proposed design and layout;
- (b) such modification to private street provisions will result in a private street network that will ensure pedestrian and vehicular mobility and safety and will be well integrated with the surrounding #streets#; and
- (c) such modification will result in a superior urban design relationship with surrounding #buildings# and open areas, including #streets# and private streets.

The Commission may prescribe appropriate conditions and controls to enhance the relationship of such publicly accessible open spaces and private streets to surrounding #buildings# and open areas.

136-52 **Authorization to Modify Bulk Regulations**

The City Planning Commission may authorize modifications of height and setback regulations, #yard# regulations, and regulations governing the minimum required distance between #buildings# and the minimum required distance between #legally required windows# and walls or #lot lines#, provided that the maximum building heights established in Sections 136-314 (Maximum building height) and 136-315 (Maximum building height and horizontal dimension for tall buildings) shall not be modified. The Commission shall find that such modifications:

- (a) will aid in achieving the general purposes and intent of this Chapter as set forth in Section 136-00 (GENERAL PURPOSES);
- (b) will provide a better distribution of #bulk# on the #zoning lot#, resulting in a superior site plan, in which the #buildings# subject to this authorization and any associated open areas will relate harmoniously with one another and with adjacent #buildings# and open areas;

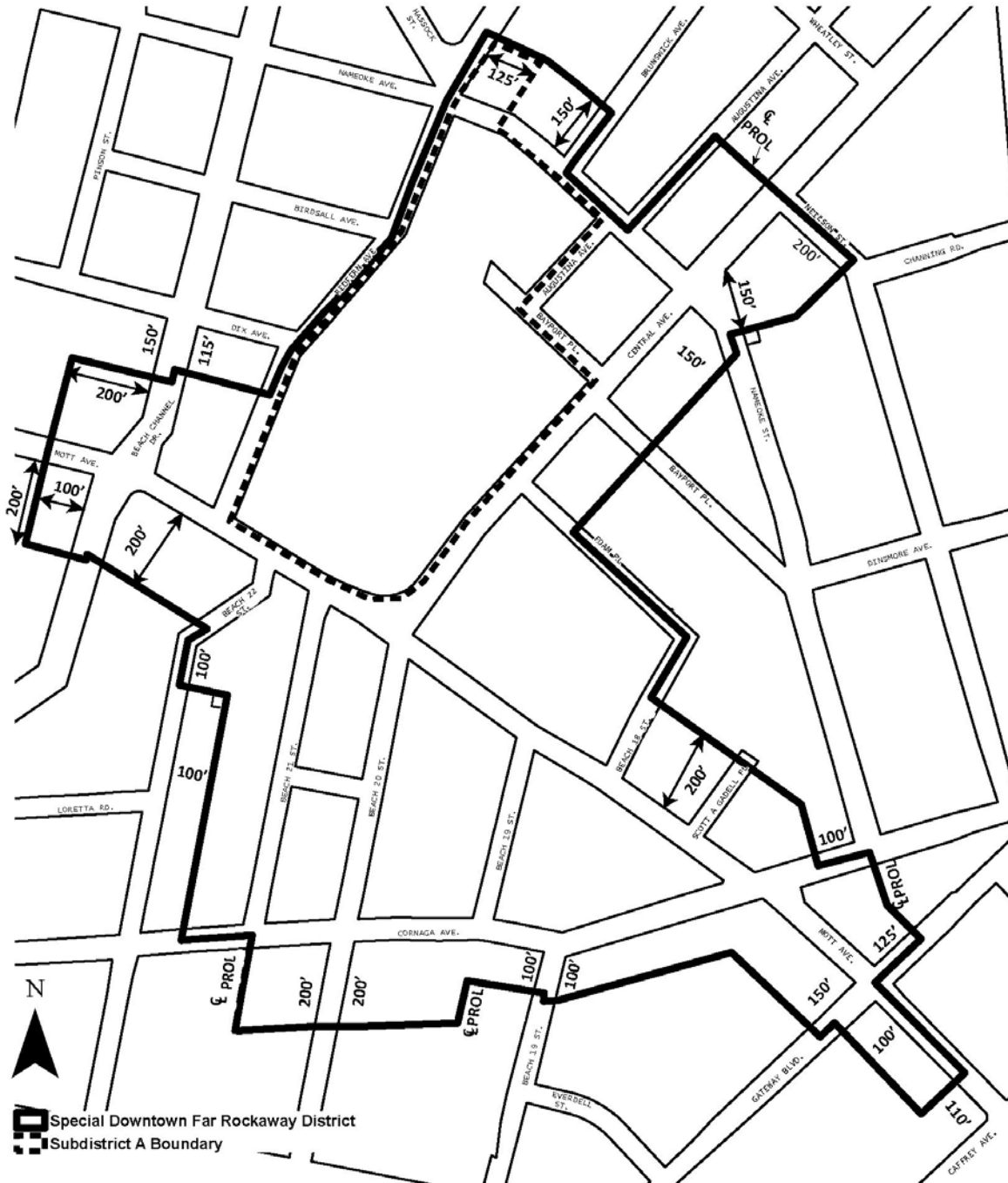
(c) will not unduly increase the #bulk# of any #building# or unduly obstruct access of adequate light and air to the detriment of the occupants or users of #buildings# on the #block# or nearby #blocks#, or of people using the public #streets# and other public spaces; and

(d) will not create traffic congestion in the surrounding area.

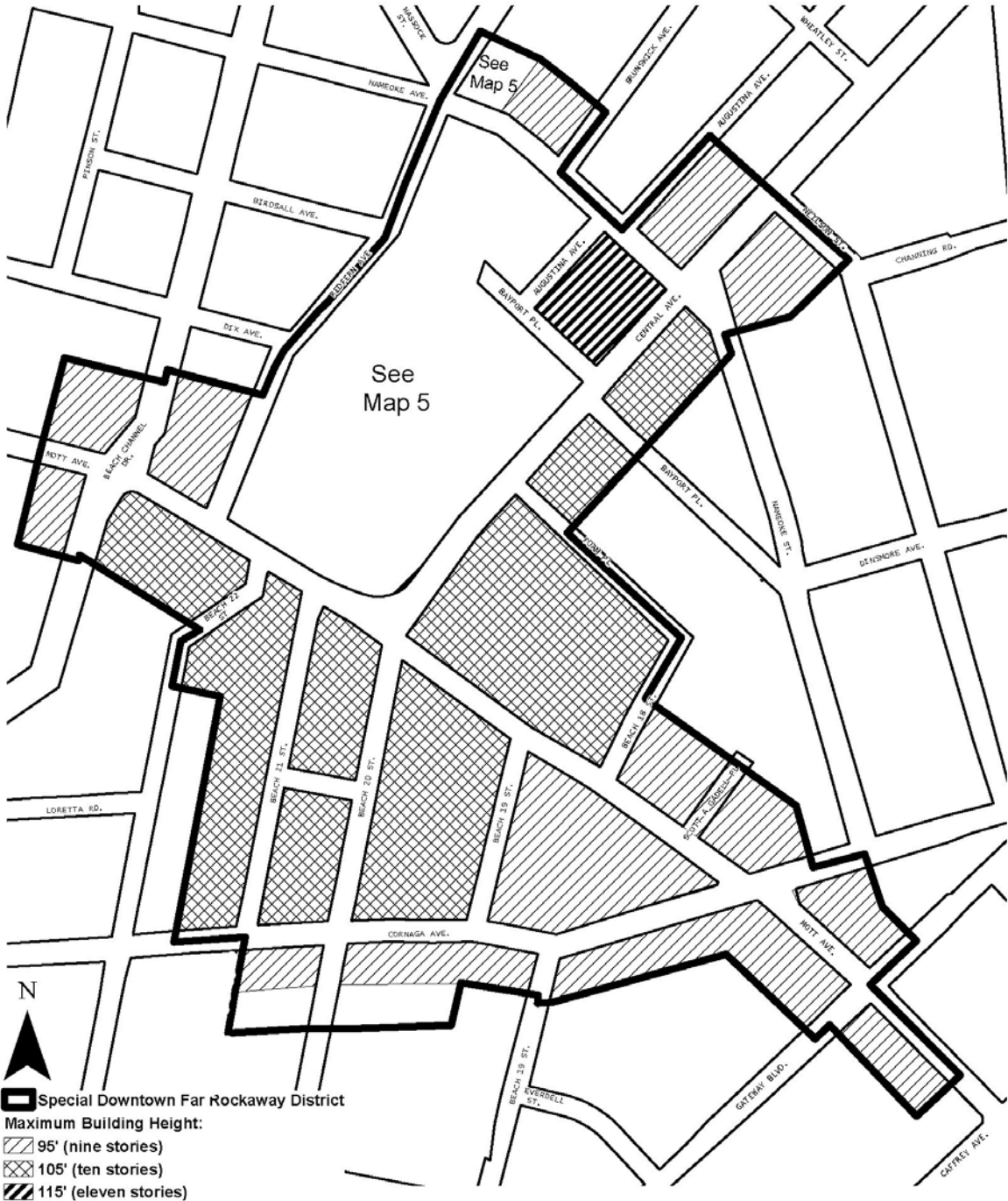
The City Planning Commission may prescribe appropriate conditions and safeguards to minimize adverse effects on the character of the surrounding area.

Appendix
Special Downtown Far Rockaway District Maps

Map 1 - Special Downtown Far Rockaway District and Subdistrict

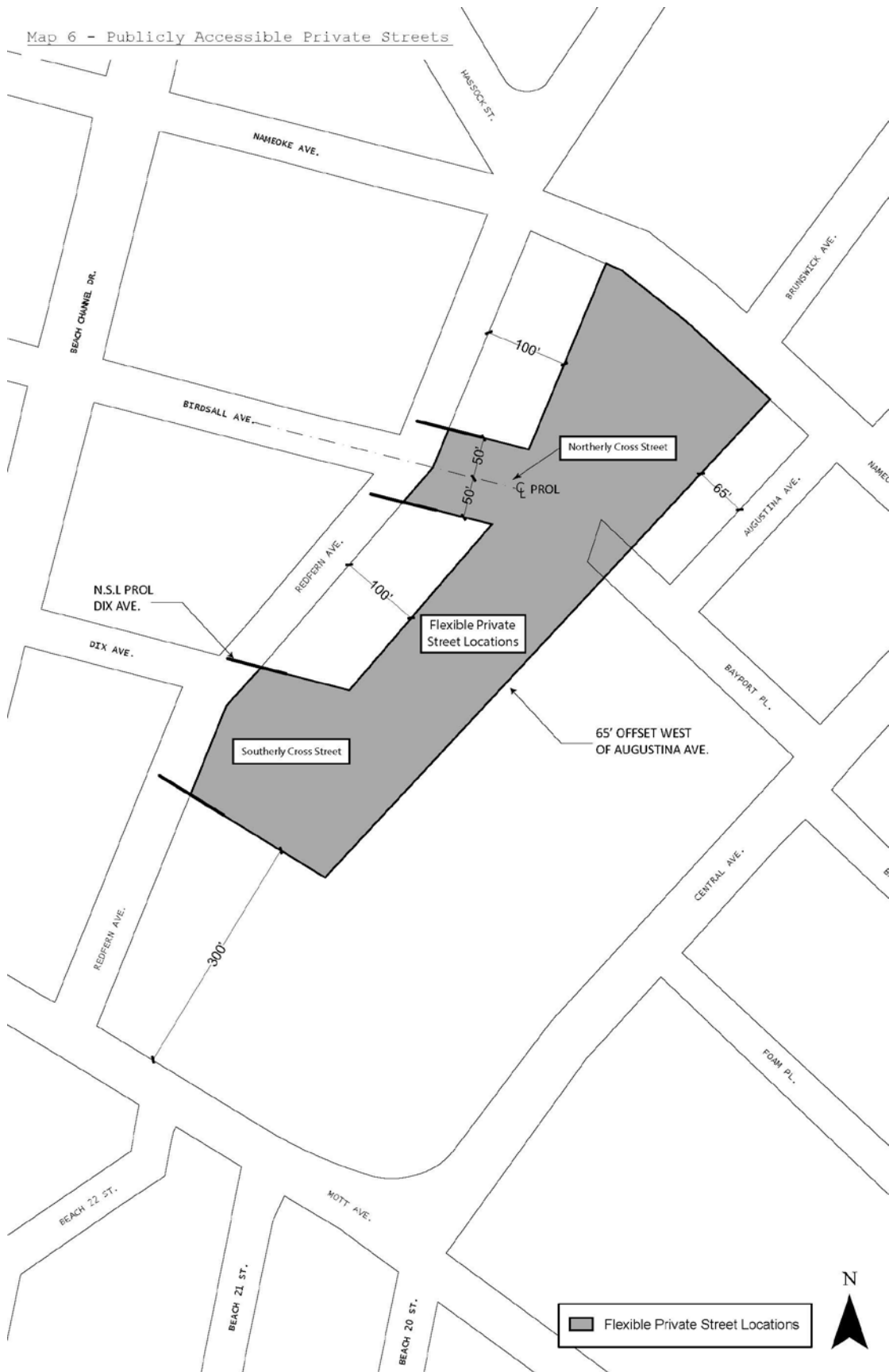


Map 4 - Maximum Building Height

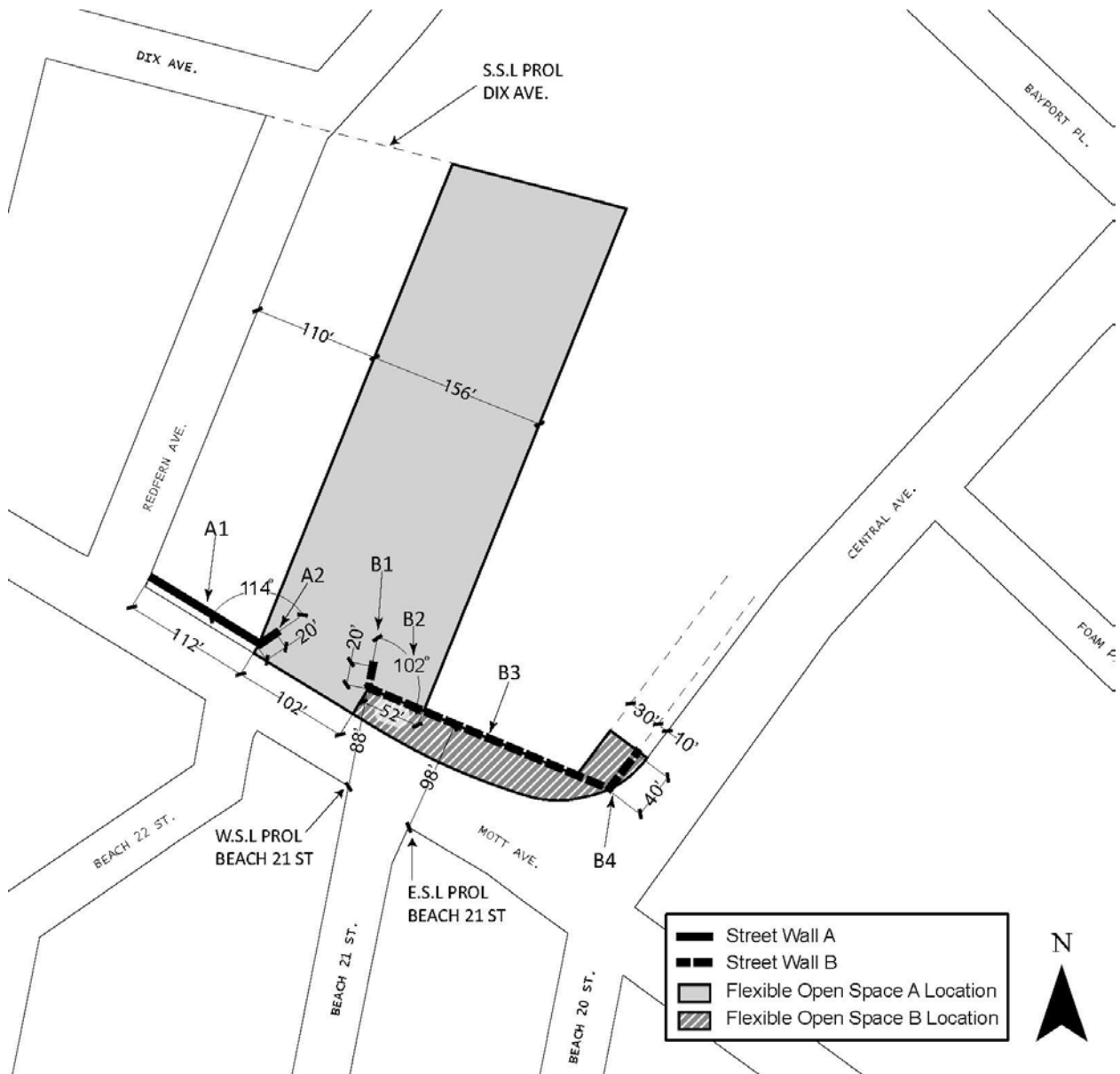


 Special Downtown Far Rockaway District
 Maximum Building Height:
 95' (nine stories)
 105' (ten stories)
 115' (eleven stories)

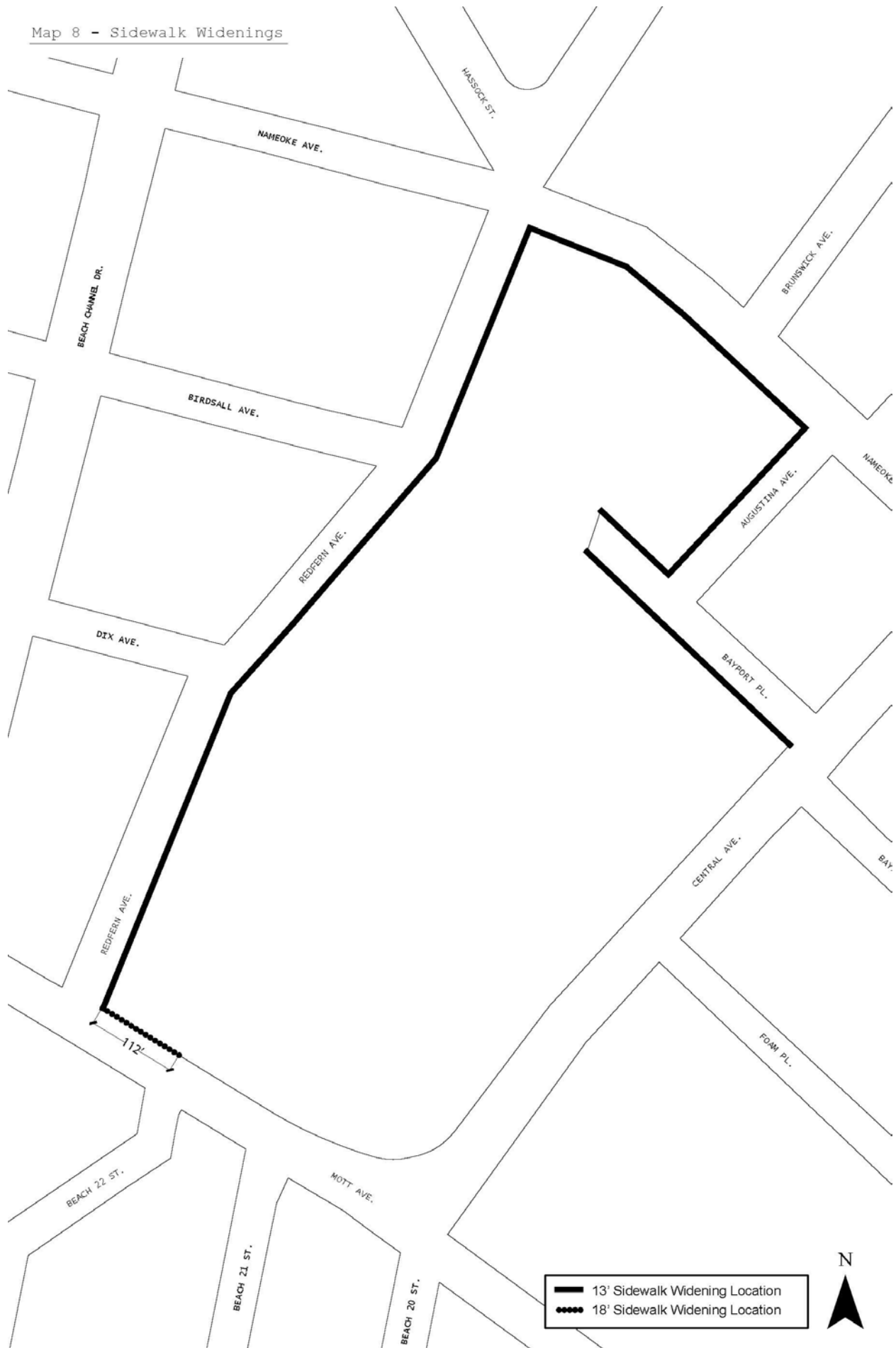
Map 6 - Publicly Accessible Private Streets



Map 7 - Mandatory Street Walls and Flexible Public Open Space Locations



Map 8 - Sidewalk Widening



* * *



— 13' Sidewalk Widening Location
..... 18' Sidewalk Widening Location

**APPENDIX F
Inclusionary Housing Designated Areas and Mandatory Inclusionary Housing Areas**

* * *

Queens

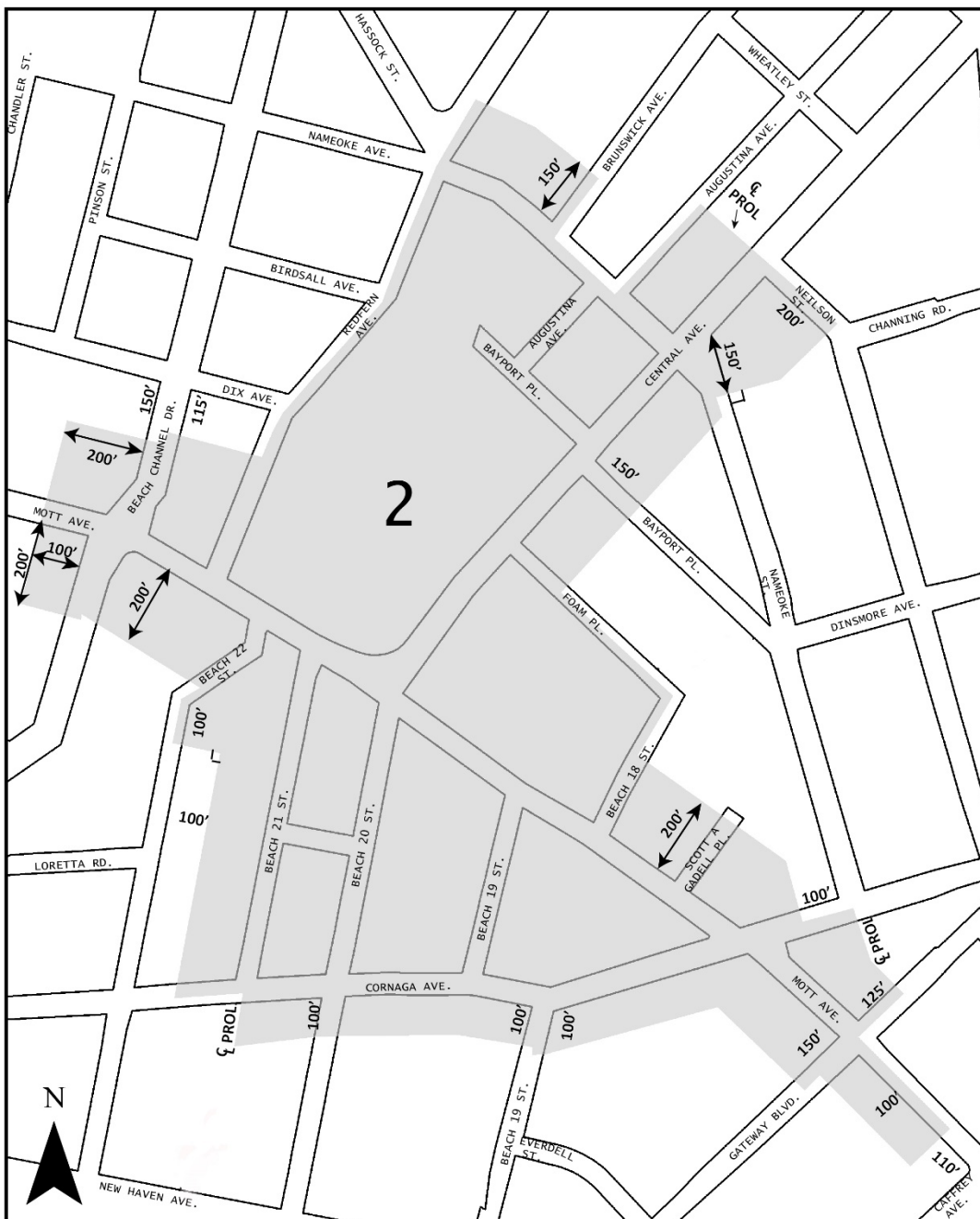
* * *

Queens Community District 14

In portions of the #Special Downtown Far Rockaway District# and in the R6, R6A, and R7-1 Districts within the areas shown on the following Maps 1 and 2:

* * *

Map 2 – [date of adoption]



■ Mandatory Inclusionary Housing area see Section 23-154(d)(3)
Area 2 [date of adoption] – MIH Program Option 1 and Option 2
Portion of Community District 14, Queens

Appendix B

Amended RWCDS Detailed Tables

Amended Reasonable Worst-Case Development Scenario (RWCDs) Detailed Tables

PROJECTED DEVELOPMENT SITES																																						
Site Info					Existing and No Action Conditions												Future With Action Conditions										Proposed Actions Increment (difference between No Action and With Action Conditions)											
Site ID (see Figure 3)	Block	Lot	Address	Street Type	Lot Area (sf) within Rezoning Area	Zoning		FAR				Area					Residential (Dus)	Use	Zoning		FAR			Development Scenario					Commercial (SF)	Community Facility (SF)	Residential (SF)	Residential (DUs)	Total (SF)	Commercial (SF)	Community Facility (SF)	Residential (SF)	Residential (DUs)	Total (SF)
						Existing Zoning	Existing Overlay	Residential FAR	Commercial FAR	Community Facility FAR	Built FAR	Total Built Area	Commercial Area	Community Facility Area	Industrial Area	Residential Area			Proposed Zoning	Proposed Overlay	Residential FAR	Commercial FAR	Com Fac FAR	Commercial (SF)	Community Facility (SF)	Residential (SF)	Residential (DUs)	Total (SF)										
1	15661	80	13-15 BEACH CHANNEL DRIVE	Wide	14,995	R5	C1-2	1.25	1	2	0.16	2,400	2,400	0	0	0	0	0	0	Laundromat	R6	C2-4	3.6	2	4.8	-	6,290	46,000	46	52,290	(2,400)	6,290	46,000	46	49,890			
2	15528	9	13-24 BEACH CHANNEL DRIVE	Wide	11,500	R5	C1-2	1.25	1	2	0.31	3,600	3,600	0	0	0	0	0	0	Car Wash	R6	C2-4	3.6	2	4.8	6,290	-	30,000	30	36,290	2,690	-	30,000	30	32,690			
3	15705	140	10-74 BEACH 22 STREET	Narrow	13,710	C8-1		0	1	2.4	0.06	840	0	0	0	0	0	0	0	Vacant	R6	C2-4	3.6	2	4.8	4,845	-	42,900	43	47,745	4,845	-	42,900	43	46,905			
4	15705	6	21-12 CORNAGA AVENUE	Narrow	8,668	C8-1		0	1	2.4	0	0	0	0	0	0	0	0	0	Vacant	R6	C2-4	3.6	2	4.8	17,680	-	103,300	103	120,980	17,680	-	103,300	103	120,980			
	15705	78	10-27 BEACH 21 STREET	Narrow	14,060	C8-1		0	1	2.4	0	0	0	0	0	0	0	0																				
	15705	81	10-17 BEACH 21 STREET	Narrow	4,770	C8-1		0	1	2.4	0	0	0	0	0	0	0	0																				
	15705	84	BEACH 21 STREET	Narrow	6,570	C8-1		0	1	2.4	0	0	0	0	0	0	0	0																				
5	15705	1	10-09 CORNAGA AVENUE	Narrow	9,968	C8-1		0	1	2.4	0.16	1,634	1,634	0	0	0	0	0	0	Auto Repair	R6	C2-4	3.6	2	4.8	7,650	-	43,800	44	51,450	6,016	-	43,800	44	49,816			
	15705	88	BEACH 21 STREET	Narrow	4,320	C8-1		0	1	2.4	0	0	0	0	0	0	0	0																				
6	15561	8	10-18 BEACH 20 STREET	Narrow	6,550	C4-2		2.43	3.4	4.8	0	0	0	0	0	0	0	0	0	Vacant	R6	C2-4	3.6	2	4.8	25,925	-	171,600	172	197,525	25,925	-	171,600	172	197,525			
	15561	10	10-25 BEACH 20 STREET	Narrow	26,158	R5/C4-2	C2-2	1.25	1	2	0	0	0	0	0	0	0	0																				
	15561	58	CORNAGA AVENUE	Narrow	22,165	R5/C4-2	C2-2	1.25	1	2	0	0	0	0	0	0	0	0																				
7	15560	8	10-16 BEACH 19 STREET	Narrow	18,694	R5	C2-2	1.25	1	2	0.7	13,000	0	0	0	0	0	0	0	Vacant Building	R6	C2-4	3.6	2	4.8	7,269	-	58,000	58	65,269	7,269	-	58,000	58	52,269.20			
8	15564	40	19-19 CORNAGA AVENUE	Narrow	5,856	C4-2		2.43	3.4	4.8	0.22	1,296	0	0	0	0	432	1	Vacant/ Residential	R5/R6	C2-4	1.25/3.6	2	4.8	5,610	-	37,300	37	42,910	5,610	-	36,868	36	41,614				
	15564	42	19-15 CORNAGA AVENUE	Narrow	9,400	R5		1.25	1	2	0	0	0	0	0	0	0	0																				
9	15563	31	GATEWAY BOULEVARD	Narrow	13,900	R5	C2-2	1.25	1	2	0	0	0	0	0	0	0	0	Vacant	R6	C2-4	3.6	2	4.8	8,000	-	133,051	133	141,051	8,000	-	133,051	133	141,051				
	15563	40	GATEWAY BOULEVARD	Narrow	17,591	R5	C2-2	1.25	1	2	0	0	0	0	0	0	0	0																				
	15563	43	GATEWAY BOULEVARD	Narrow	7,690	R5	C2-2	1.25	1	2	0	0	0	0	0	0	0	0																				
10	15559	8	MOTT AVENUE	Narrow	19,320	C4-2		2.43	3.4	4.8	0	0	0	0	0	0	0	0	Parking	R6	C2-4	3.6	2	4.8	5,525	3,500	62,500	63	71,525	5,525	3,500	62,500	63	71,525				
11	15559	58	11-19 FOAM PLACE	Narrow	10,176	C4-2		2.43	3.4	4.8	0	0	0	0	0	0	0	0	Vacant	R6	C2-4	3.6	0	4.8	3,485	-	33,000	33	36,485	3,485	-	33,000	33	36,485				
12	15559	62	FOAM PLACE	Narrow	10,431	C4-2		2.43	3.4	4.8	0	0	0	0	0	0	0	0	Vacant	R6	C2-4	3.6	0	4.8	3,412	-	34,100	34	37,512	3,412	-	34,100	34	37,512				
13	15544	34	14-19 CENTRAL AVENUE	Narrow	18,750	C8-1		0	1	2.4	0.09	1,710	1,710	0	0	0	0	0	0	Auto Repair	R6	C2-4	3.6	2	4.8	6,970	-	60,400	60	67,370	5,260	-	60,400	60	65,660			
14	15543	43	CENTRAL AVENUE	Narrow	9,926	R5	C1-2	1.25	1	2	0	0	0	0	0	0	0	0	0	Vacant	R6	C2-4	3.6	2	4.8	-	2,805	21,600	22	24,405	-	2,805	21,600	22	24,405			
15	15536	12	AUGUSTINA AVENUE	Narrow	5,000	C8-1		0	1	2.4	0	0	0	0	0	0	0	0	Church/ Vacant	R7-1	C2-4	4.6	2	4.8	-	25,500	138,000	138	163,500	-	20,500	138,000	138	163,500				
	15536	15	AUGUSTINA AVENUE	Narrow	5,000	C8-1		0	1	2.4	0	0	0	0	0	0	0	0																				
	15536	18	19-15 NAMEOKE AVENUE	Narrow	5,000	C8-1		0	1	2.4	0.63	3,150	0	0	0	0	0	0																				
	15536	22	14-14 CENTRAL AVENUE	Narrow	10,000	C8-1		0	1	2.4	0.5	5,000	0	5000	0	0	0	0																				
	15536	28	CENTRAL AVENUE	Narrow	5,000	C8-1		0	1	2.4	0	0	0	0	0	0	0	0																				
16	15536	6	13-12 BAYPORT PLACE	Narrow	10,000	C8-1		0	1	2.4	2	20,000	0	0	0	0	0	0	Vacant Building	R7-1	C2-4	4.6	2	4.8	7,183	-	54,300	54	61,483	7,183	-	54,300	54	41,483				
	15536	31	CENTRAL AVENUE	Narrow	4,500	C8-1		0	1	2.4	0	0	0	0	0	0	0	0																				
17	15529	161	14-14 BRUNSWICK AVENUE	Narrow	32,778	M1-1		0	1	2.4	0.17	5,708	0	0	5708	0	0	0	Vacant*	M1-1/R6	C2-4	1/3.6	2	4.8	5,525	-	32,400	32	37,925	5,525	-	32,400	32	32,217				
18	15574	36	GATEWAY BOULEVARD	Narrow	19,362	R-5		1.25	0	2	0	0	0	0	0	0	0	0	0	Vacant	R6	C2-4	3.6	2	4.8	-	-	70,814	71	70,814	-	-	70,814	71	70,814			
19	15535	11	13-25 AUGUSTINA AVENUE	Narrow	4,408	R-5	C1-2	1.25	1	2	0.05	200	200	0	0	0	0	0	0	Storage	R6	C2-4	3.6	2	4.8	7,820	-	26,550	27	34,370	7,620	-	26,550	27	34,170			
	15535	58	13-20 CENTRAL AVENUE	Narrow	1,309	R-5	C1-2	1.25	1	2	0.78	1,020	1,020	0	0	0	0	0	Laundromat	R6	C2-4	3.6	2	4.8														
	15535	59	13-22 CENTRAL AVENUE	Narrow	1,264	R-5	C1-2	1.25	1	2	0.81	1,020	1,020	0	0	0	0	0	0	Supermarket	R6	C2-4	3.6	2	4.8													
	15535	60	13-24 CENTRAL AVENUE	Narrow	1,277	R-5	C1-2	1.25	1	2	0.75	960	960	0	0	0	0	0	0	Supermarket	R6	C2-4	3.6	2	4.8													
	15535	61	13-26 CENTRAL AVENUE	Narrow	1,290	R-5	C1-2	1.25	1	2	0.74	960	960	0	0	0	0	0	0	Supermarket	R6	C2-4	3.6	2	4.8													
TOTALS FOR PROJECTED SITES					391,356							62,498	13,504	5,000	5,708	432	1							123,189	38,095	1,199,615	1,200	1,360,899	113,645	33,095	1,199,183	1,199	1,298,401					

Amended Reasonable Worst-Case Development Scenario (RWCDs) Detailed Tables (cont.)

PROPOSED DOWNTOWN FAR ROCKAWAY URBAN RENEWAL AREA (DFRURA)																																												
Site Info					Existing and No Action Conditions													Future With Action Condition																										
Site ID (see Figure 3)	Block	Lot	Address	Street Type	Lot Area (sf) within Development Site	Zoning		FAR				Area					Residential (Dus)	Use	Zoning		FAR			Development Scenario					Proposed Actions Increment (difference between No Action and With Action Conditions)															
						Existing Zoning	Existing Overlay	Residential FAR	Commercial FAR	Community Facility FAR	Built FAR	Total Built Area	Commercial Area	Community Facility Area	Industrial Area	Residential Area			Proposed Zoning	Proposed Overlay	Residential FAR	Commercial FAR	Com Fac FAR	Commercial (SF)	Community Facility (SF)	Residential (SF)	Residential (DUs)	Total (SF)	Commercial (SF)	Community Facility (SF)	Residential (SF)	Residential (DUs)	Total (SF)											
DFRURA	15529	9	NAMEOKE AVENUE	Narrow	526	M1-1		0.00	1.00	2.40	0.00	0	0	0	0	0	0	0	Vacant	R6	C2-4	3.6	2	4.8	129,077	36,295	1,766,666	1,747	1,932,038	47,489	36,295	1,759,453	1,740	1,767,388										
	15529	10	20-50 NAMEOKE AVENUE	Narrow	17,812	M1-1		0.00	1.00	2.40	0.85	15,085	0	0	0	0	0	0	Vacant Building	R6	C2-4	3.6	2	4.8																				
	15537	1	20-02 MOTT AVENUE	Narrow	267,496	C4-2/C8-1		2.43	3.40	4.80	0.14	78,750	56,100	0	0	0	0	0	Shopping Center	R7-1	C2-4	4.6	2	4.8																				
	15537	5	20-10 MOTT AVENUE	Narrow	11,100	C4-2		2.43	3.40	4.80	0.70	7,752	7,752	0	0	0	0	0	Bank	R7-1	C2-4	4.6	2	4.8																				
	15537	40	18-01 REDFERN AVENUE	Narrow	4,600	C8-1		0.00	1.00	2.40	0.27	0	0	0	0	0	0	0	Church	R7-1	C2-4	4.6	2	4.8																				
	15537	46	REDFERN AVENUE	Narrow	5,353	C8-1		0.00	1.00	2.40	0.00	0	0	0	0	0	0	0	Vacant	R7-1	C2-4	4.6	2	4.8																				
	15537	50	17-27 REDFERN AVENUE	Narrow	2,974	C8-1		0.00	1.00	2.40	0.00	0	0	0	0	0	0	0	Vacant	R7-1	C2-4	4.6	2	4.8																				
	15537	51	17-25 REDFERN AVENUE	Narrow	3,018	C8-1		0.00	1.00	2.40	0.00	0	0	0	0	0	0	0	Vacant	R7-1	C2-4	4.6	2	4.8																				
	15537	53	17-21 REDFERN AVENUE	Narrow	3,143	C8-1		0.00	1.00	2.40	0.45	1,407	0	0	0	1,407	2	Residential	R7-1	C2-4	4.6	2	4.8																					
	15537	54	17-19 REDFERN AVENUE	Narrow	2,352	C8-1		0.00	1.00	2.40	0.68	1,594	0	0	0	1,594	1	Residential	R7-1	C2-4	4.6	2	4.8																					
	15537	55	REDFERN AVENUE	Narrow	2,600	C8-1		0.00	1.00	2.40	0.00	0	0	0	0	0	0	0	Vacant	R7-1	C2-4	4.6	2	4.8																				
	15537	56	17-15 REDFERN AVENUE	Narrow	2,591	C8-1		0.00	1.00	2.40	0.62	1,594	0	0	0	1,594	2	Residential	R7-1	C2-4	4.6	2	4.8																					
	15537	57	17-11 REDFERN AVENUE	Narrow	2,581	C8-1		0.00	1.00	2.40	0.40	1,024	0	0	0	1,024	1	Residential	R7-1	C2-4	4.6	2	4.8																					
	15537	58	17-09 REDFERN AVENUE	Narrow	2,566	C8-1		0.00	1.00	2.40	0.62	1,594	0	0	0	1,594	1	Residential	R7-1	C2-4	4.6	2	4.8																					
	15537	59	REDFERN AVENUE	Narrow	2,295	C8-1		0.00	1.00	2.40	0.00	0	0	0	0	0	0	0	Vacant	R7-1	C2-4	4.6	2	4.8																				
	15537	60	17-01 REDFERN AVENUE	Narrow	6,148	C8-1		0.00	1.00	2.40	0.37	2,275	0	0	0	2,275	0	0	Recycling Center	R7-1	C2-4	4.6	2	4.8																				
	15537	63	16-29 REDFERN AVENUE	Narrow	4,667	C8-1		0.00	1.00	2.40	0.96	4,500	0	0	4,500	0	0	0	Recycling Center	R7-1	C2-4	4.6	2	4.8																				
	15537	65	16-25 REDFERN AVENUE	Narrow	15,610	C8-1		0.00	1.00	2.40	0.13	2,054	0	0	2,054	0	0	0	Recycling Center	R7-1	C2-4	4.6	2	4.8																				
	15537	71	20-47 NAMEOKE AVENUE	Narrow	20,280	C8-1		0.00	1.00	2.40	1.44	29,285	0	0	29,285	0	0	0	Industrial	R7-1	C2-4	4.6	2	4.8																				
	15537	79	NAMEOKE AVENUE	Narrow	620	C8-1		0.00	1.00	2.40	0.00	0	0	0	0	0	0	0	Vacant	R7-1	C2-4	4.6	2	4.8																				
	15537	89	20-09 NAMEOKE AVENUE	Narrow	5,356	C8-1		0.00	1.00	2.40	1.18	6,300	6,300	0	0	0	0	0	Industrial	R7-1	C2-4	4.6	2	4.8																				
	15537	92	NAMEOKE AVENUE	Narrow	2,013	C8-1		0.00	1.00	2.40	0.09	172	172	0	0	0	0	0	Auto Repair	R7-1	C2-4	4.6	2	4.8																				
	15537	94	14-02 AUGUSTINA AVENUE	Narrow	4,850	C8-1		0.00	1.00	2.40	1.00	4,850	4,850	0	0	0	0	0	Auto Repair	R7-1	C2-4	4.6	2	4.8																				
	15537	99	14-06 AUGUSTINA AVENUE	Narrow	8,279	C8-1		0.00	1.00	2.40	0.77	6,414	6,414	0	0	0	0	0	Auto Repair	R7-1	C2-4	4.6	2	4.8																				
	15537	100	AUGUSTINA AVENUE	Narrow	20,483	C8-1		0.00	1.00	2.40	0.00	0	0	0	0	0	0	0	Vacant	R7-1	C2-4	4.6	2	4.8																				
	15537	112	BAYPORT PLACE	Narrow	17,568	C4-2		2.43	3.40	4.80	0.00	0	0	0	0	0	0	0	Vacant	R7-1	C2-4	4.6	2	4.8																				
	15537	128	CENTRAL AVENUE	Narrow	5,000	C4-2		2.43	3.40	4.80	0.00	0	0	0	0	0	0	0	Vacant	R7-1	C2-4	4.6	2	4.8																				
	15537	130	CENTRAL AVENUE	Narrow	4,958	C4-2		2.43	3.40	4.80	0.00	0	0	0	0	0	0	0	Vacant	R7-1	C2-4	4.6	2	4.8																				
	TOTALS FOR PROPOSED DFRURA					446,839							164,650	81,588	0	38,114	7,213	7																	129,077	36,295	1,766,666	1,747	1,932,038	47,489	36,295	1,759,453	1,740	1,767,388

Amended Reasonable Worst-Case Development Scenario (RWCDs) Detailed Tables (cont.)

DISPOSITION SITES																																	
Site Info					Existing and No Action Conditions													Future With Action Condition										Proposed Actions Increment (difference between No Action and With Action Conditions)					
Site ID (See Figure 3)	Block	Lot	Address	Street Type	Lot Area (sf)	Zoning		FAR				Area					Residential (Dus)	Use	Zoning		FAR			Development Scenario					Commercial (SF)	Community Facility (SF)	Residential (SF)	Residential (DUs)	Total (SF)
						Existing Zoning	Existing Overlay	Residential FAR	Commercial FAR	Community Facility FAR	Built FAR	Total Built Area	Commercial Area	Community Facility Area	Industrial Area	Residential Area			Proposed Zoning	Proposed Overlay	Residential FAR	Commercial FAR	Com Fac FAR	Commercial (SF)	Community Facility (SF)	Residential (SF)	Residential (DUs)	Total (SF)					
DOT/MTA Disposition Site	15705	59	BEACH 21 STREET	Narrow	48565	C8-1/RS		0.00	1.00	2.40	0.00	0	0	0	0	0	0	0	R6	C2-4	3.6	2	4.8	7,421	11,557	177,238	176	196,216	7,421	11,557	177,238	176	196,216
	15705	69	BEACH 21 STREET	Narrow	9479	C8-1		0.00	1.00	2.40	0.00	0	0	0	0	0	0	0	R6	C2-4	3.6	2	4.8										
DSNY Disposition Site	15534	70			14000	R3X						0	0	0	0	0	0	0	NA				0	0	8,000	8	8,000	-	-	8,000	8	8,000	
TOTAL FOR DISPOSITION SITES					72,044							0	0	0	0	0	0	0						7,421	11,557	185,238	184	204,216	7,421	11,557	185,238	184	204,216

Amended Reasonable Worst-Case Development Scenario (RWCDs) Detailed Tables (cont.)

POTENTIAL DEVELOPMENT SITES																																		
Site Info					Existing and No Action Conditions												Future With-Action Conditions										Proposed Actions Increment (difference between No Action and With Action Conditions)							
Site ID (see Figure 3)	Block	Lot	Address	Street Type	Lot Area (sf) within Reasoning Area	Zoning		FAR					Area					Residential (Dus)	Use	Zoning		FAR			Development Scenario					Commercial (SF)	Community Facility (SF)	Residential (SF)	Residential (DUs)	Total (SF)
						Existing Zoning	Existing Overlay	Residential FAR	Commercial FAR	Community Facility FAR	Built FAR	Total Built Area	Commercial Area	Community Facility Area	Industrial Area	Residential Area	Proposed Zoning			Proposed Overlay	Residential FAR	Commercial FAR	Com Fac FAR	Commercial (SF)	Community Facility (SF)	Residential (SF)	Residential (DUs)	Total (SF)						
A	15661	1	22-02 MOTT AVENUE	Wide	12,112	RS	C1-2	1.25	1	2	0.13	1,601	1,601	0	0	0	0	Restaurant	R6	C2-4	3.6	2	4.8	5,610	-	37,800	38	43,410	4,009	-	37,800	38	41,809	
B	15709	101	21-41 MOTT AVENUE	Wide	19,600	RS	C1-2	1.25	1	2	0.21	4,100	4,100	0	0	0	0	Restaurant	R6	C2-4	3.6	2	4.8	8,075	-	62,000	62	70,075	3,975	-	62,000	62	65,975	
C	15709	109	21-23 MOTT AVENUE	Narrow	21,278	RS	C1-2	1.25	0	2	0.23	6,050	6,050	0	0	0	0	Retail/Restaurant	R5/R6	C2-4	1.25/3.6	1.0/2.0	4.8	9,350	-	85,000	85	94,350	3,300	-	85,000	85	88,300	
D	15528	5	13-12 BEACH CHANNEL DRIVE	Narrow	10,500	RS	C1-2	1.25	1	2	0.13	1,400	1,400	0	0	0	0	Restaurant	R6	C2-4	3.6	2	4.8	3,825	-	28,400	28	32,225	2,425	-	28,400	28	30,825	
E	15564	45	19-03 CORNAGA AVENUE	Narrow	11,107	RS		1.25		2	0.82	9,150	9,150	0	0	0	0	Retail	R6	C2-4	3.6	2	4.8	5,865	-	34,000	34	39,865	(3,285)	-	34,000	34	30,715	
F	15561	34	19-01 MOTT AVENUE	Narrow	23,048	RS/C4-2	C2-2	1.25	0	2	0.45	10,400	10,400	0	0	0	0	Retail	R6	C2-4	3.6	2	4.8	10,880	5,500	66,400	66	82,780	480	5,500	66,400	66	72,380	
G	15544	40	14-01 CENTRAL AVENUE	Narrow	10,950	C8-1		0	1	2.4	0.13	1,440	1,440	0	0	0	0	Retail	R6	C2-4	3.6	2	4.8	4,675	-	34,700	35	39,375	3,235	-	34,700	35	37,935	
H	15535	1	13-38 CENTRAL AVENUE	Narrow	10,450	RS	C1-2	1.25	0	2	0.31	3,200	3,200	0	0	0	0	Retail/Restaurant	R6	C2-4	3.6	2	4.8	5,440	-	29,600	30	35,040	2,240	-	29,600	30	31,840	
I	15559	12	19-30 MOTT AVENUE	Narrow	60,650	C4-2		2.43	3.4	4.8	0.39	23,368	23,368	0	0	0	0	Retail/Parking	R6	C2-4	3.6	2	4.8	34,775	-	174,360	174	209,135	11,407	-	174,360	174	185,767	
	15559	54	FOAM PLACE	Narrow	10,150	C4-2		2.43	3.4	4.8	0	0	0	0	0	0																		
TOTAL FOR POTENTIAL SITES					189,845							60,709	60,709	0	0	0	0						88,495	5,500	552,260	552	646,255	27,786	5,500	552,260	552	585,546		

Appendix C

New York City Waterfront Revitalization Program Consistency Assessment Form

**for
Downtown Far Rockaway Redevelopment Project
A-Application**

NEW YORK CITY WATERFRONT REVITALIZATION PROGRAM Consistency Assessment Form

Proposed actions that are subject to CEQR, ULURP or other local, state or federal discretionary review procedures, and that are within New York City's Coastal Zone, must be reviewed and assessed for their consistency with the [New York City Waterfront Revitalization Program](#) (WRP) which has been approved as part of the State's Coastal Management Program.

This form is intended to assist an applicant in certifying that the proposed activity is consistent with the WRP. It should be completed when the local, state, or federal application is prepared. The completed form and accompanying information will be used by the New York State Department of State, the New York City Department of City Planning, or other city or state agencies in their review of the applicant's certification of consistency.

A. APPLICANT INFORMATION

Name of Applicant: New York City Economic Development Corporation

Name of Applicant Representative: Nathan Gray, Vice President

Address: 110 William Street, New York, NY 10038

Telephone: 212-312-3718 Email: ngray@edc.nyc

Project site owner (if different than above): _____

B. PROPOSED ACTIVITY

If more space is needed, include as an attachment.

I. Brief description of activity

The City of New York, acting through the New York City (NYC) Economic Development Corporation (EDC), the New York City Department of Housing Preservation and Development (HPD), and the New York City Department of Citywide Administrative Services (DCAS), is proposing a series of land use actions, including zoning map amendments, zoning text amendments, and the designation and approval of an Urban Renewal Area (URA) and Plan to implement recommendations of a comprehensive plan to redevelop and revitalize an approximately 22-block area of the Downtown Far Rockaway neighborhood of Queens, Community District 14. The Proposed Actions based on the A-Application are expected to result in a net increase of 3,123 dwelling units (DUs), 168,555 gross square feet (gsf) of retail space and 80,947 gsf of community facility space (the "Proposed Project"). The Proposed Project also would provide a new publicly-accessible open space.

2. Purpose of activity

In 2014, an inter-agency effort started, with the goal of coordinating City projects and investments in the Rockaway peninsula post-Hurricane Sandy. The Downtown Far Rockaway Working Group (the Working Group) was convened in October 2015 by the council member representing City Council District 31 (which includes Downtown Far Rockaway), in partnership with City Hall to catalyze the revitalization of the peninsula and Downtown Far Rockaway. In support of this effort, the City looked across the peninsula at opportunities to provide new affordable housing and support neighborhood growth with a specific focus on Downtown Far Rockaway, the Peninsula's historic downtown core. The Working Group included local elected officials and representatives from the community, business, and nonprofit sectors. With input from the public, the Working Group developed a set of recommendations to guide future public and private investment in Downtown Far Rockaway. The recommendations, delivered to Mayor de Blasio on February 1, 2016, were organized around the following goals: (1) Re-establish Downtown Far Rockaway as the commercial and transportation hub of the Rockaway peninsula; (2) Reposition the area as a mixed-use district, including new mixed-income housing; (3) Activate the public realm with new connections and public open space; (4) Improve the quality of life for residents through access to community services, education and quality jobs; and (5) Build the capacity of community organizations and support local businesses. In 2016, Mayor de Blasio announced a \$91 million commitment for the Downtown Far Rockaway area, and referenced initiatives including helping businesses along commercial corridors, job training workshops, an improved recreational center, and a new library, all part of comprehensive revitalization effort for the neighborhood.

C. PROJECT LOCATION

Borough: Queens Tax Block/Lot(s): NA

Street Address: NA

Name of water body (if located on the waterfront): NA

D. REQUIRED ACTIONS OR APPROVALS

Check all that apply.

City Actions/Approvals/Funding

City Planning Commission Yes No

<input type="checkbox"/> City Map Amendment	<input type="checkbox"/> Zoning Certification	<input type="checkbox"/> Concession
<input checked="" type="checkbox"/> Zoning Map Amendment	<input type="checkbox"/> Zoning Authorizations	<input type="checkbox"/> UDAAP
<input checked="" type="checkbox"/> Zoning Text Amendment	<input type="checkbox"/> Acquisition – Real Property	<input type="checkbox"/> Revocable Consent
<input type="checkbox"/> Site Selection – Public Facility	<input type="checkbox"/> Disposition – Real Property	<input type="checkbox"/> Franchise
<input type="checkbox"/> Housing Plan & Project	<input type="checkbox"/> Other, explain: _____	
<input type="checkbox"/> Special Permit		

(if appropriate, specify type: Modification Renewal other) Expiration Date: _____

Board of Standards and Appeals Yes No

<input type="checkbox"/> Variance (use)	
<input type="checkbox"/> Variance (bulk)	
<input type="checkbox"/> Special Permit	

(if appropriate, specify type: Modification Renewal other) Expiration Date: _____

Other City Approvals

<input type="checkbox"/> Legislation	<input type="checkbox"/> Funding for Construction, specify: _____
<input type="checkbox"/> Rulemaking	<input type="checkbox"/> Policy or Plan, specify: _____
<input type="checkbox"/> Construction of Public Facilities	<input type="checkbox"/> Funding of Program, specify: _____
<input type="checkbox"/> 384 (b) (4) Approval	<input type="checkbox"/> Permits, specify: _____
<input type="checkbox"/> Other, explain: _____	

State Actions/Approvals/Funding

<input type="checkbox"/> State permit or license, specify Agency: _____	Permit type and number: _____
<input type="checkbox"/> Funding for Construction, specify: _____	
<input type="checkbox"/> Funding of a Program, specify: _____	
<input type="checkbox"/> Other, explain: _____	

Federal Actions/Approvals/Funding

<input type="checkbox"/> Federal permit or license, specify Agency: _____	Permit type and number: _____
<input type="checkbox"/> Funding for Construction, specify: _____	
<input type="checkbox"/> Funding of a Program, specify: _____	
<input type="checkbox"/> Other, explain: _____	

Is this being reviewed in conjunction with a [Joint Application for Permits?](#) Yes No

E. LOCATION QUESTIONS

1. Does the project require a waterfront site? Yes No
2. Would the action result in a physical alteration to a waterfront site, including land along the shoreline, land under water or coastal waters? Yes No
3. Is the project located on publicly owned land or receiving public assistance? Yes No
4. Is the project located within a FEMA 1% annual chance floodplain? (6.2) Yes No
5. Is the project located within a FEMA 0.2% annual chance floodplain? (6.2) Yes No
6. Is the project located adjacent to or within a special area designation? See [Maps – Part III](#) of the NYC WRP. If so, check appropriate boxes below and evaluate policies noted in parentheses as part of WRP Policy Assessment (Section F).
 - Significant Maritime and Industrial Area (SMIA) (2.1)
 - Special Natural Waterfront Area (SNWA) (4.1)
 - Priority Martine Activity Zone (PMAZ) (3.5)
 - Recognized Ecological Complex (REC) (4.4)
 - West Shore Ecologically Sensitive Maritime and Industrial Area (ESMIA) (2.2, 4.2)

F. WRP POLICY ASSESSMENT

Review the project or action for consistency with the WRP policies. For each policy, check Promote, Hinder or Not Applicable (N/A). For more information about consistency review process and determination, see **Part I** of the [NYC Waterfront Revitalization Program](#). When assessing each policy, review the full policy language, including all sub-policies, contained within **Part II** of the WRP. The relevance of each applicable policy may vary depending upon the project type and where it is located (i.e. if it is located within one of the special area designations).

For those policies checked Promote or Hinder, provide a written statement on a separate page that assesses the effects of the proposed activity on the relevant policies or standards. If the project or action promotes a policy, explain how the action would be consistent with the goals of the policy. If it hinders a policy, consideration should be given toward any practical means of altering or modifying the project to eliminate the hindrance. Policies that would be advanced by the project should be balanced against those that would be hindered by the project. If reasonable modifications to eliminate the hindrance are not possible, consideration should be given as to whether the hindrance is of such a degree as to be substantial, and if so, those adverse effects should be mitigated to the extent practicable.

		Promote	Hinder	N/A
I	Support and facilitate commercial and residential redevelopment in areas well-suited to such development.	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
1.1	Encourage commercial and residential redevelopment in appropriate Coastal Zone areas.	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
1.2	Encourage non-industrial development with uses and design features that enliven the waterfront and attract the public.	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
1.3	Encourage redevelopment in the Coastal Zone where public facilities and infrastructure are adequate or will be developed.	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
1.4	In areas adjacent to SMIA's, ensure new residential development maximizes compatibility with existing adjacent maritime and industrial uses.	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
1.5	Integrate consideration of climate change and sea level rise into the planning and design of waterfront residential and commercial development, pursuant to WRP Policy 6.2.	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

		Promote	Hinder	N/A
2	Support water-dependent and industrial uses in New York City coastal areas that are well-suited to their continued operation.	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
2.1	Promote water-dependent and industrial uses in Significant Maritime and Industrial Areas.	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
2.2	Encourage a compatible relationship between working waterfront uses, upland development and natural resources within the Ecologically Sensitive Maritime and Industrial Area.	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
2.3	Encourage working waterfront uses at appropriate sites outside the Significant Maritime and Industrial Areas or Ecologically Sensitive Maritime Industrial Area.	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
2.4	Provide infrastructure improvements necessary to support working waterfront uses.	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
2.5	Incorporate consideration of climate change and sea level rise into the planning and design of waterfront industrial development and infrastructure, pursuant to WRP Policy 6.2.	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
3	Promote use of New York City's waterways for commercial and recreational boating and water-dependent transportation.	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
3.1.	Support and encourage in-water recreational activities in suitable locations.	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
3.2	Support and encourage recreational, educational and commercial boating in New York City's maritime centers.	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
3.3	Minimize conflicts between recreational boating and commercial ship operations.	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
3.4	Minimize impact of commercial and recreational boating activities on the aquatic environment and surrounding land and water uses.	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
3.5	In Priority Marine Activity Zones, support the ongoing maintenance of maritime infrastructure for water-dependent uses.	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
4	Protect and restore the quality and function of ecological systems within the New York City coastal area.	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
4.1	Protect and restore the ecological quality and component habitats and resources within the Special Natural Waterfront Areas.	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
4.2	Protect and restore the ecological quality and component habitats and resources within the Ecologically Sensitive Maritime and Industrial Area.	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
4.3	Protect designated Significant Coastal Fish and Wildlife Habitats.	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
4.4	Identify, remediate and restore ecological functions within Recognized Ecological Complexes.	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
4.5	Protect and restore tidal and freshwater wetlands.	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
4.6	In addition to wetlands, seek opportunities to create a mosaic of habitats with high ecological value and function that provide environmental and societal benefits. Restoration should strive to incorporate multiple habitat characteristics to achieve the greatest ecological benefit at a single location.	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
4.7	Protect vulnerable plant, fish and wildlife species, and rare ecological communities. Design and develop land and water uses to maximize their integration or compatibility with the identified ecological community.	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
4.8	Maintain and protect living aquatic resources.	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

		Promote	Hinder	N/A
5	Protect and improve water quality in the New York City coastal area.	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
5.1	Manage direct or indirect discharges to waterbodies.	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
5.2	Protect the quality of New York City's waters by managing activities that generate nonpoint source pollution.	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
5.3	Protect water quality when excavating or placing fill in navigable waters and in or near marshes, estuaries, tidal marshes, and wetlands.	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
5.4	Protect the quality and quantity of groundwater, streams, and the sources of water for wetlands.	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
5.5	Protect and improve water quality through cost-effective grey-infrastructure and in-water ecological strategies.	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
6	Minimize loss of life, structures, infrastructure, and natural resources caused by flooding and erosion, and increase resilience to future conditions created by climate change.	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
6.1	Minimize losses from flooding and erosion by employing non-structural and structural management measures appropriate to the site, the use of the property to be protected, and the surrounding area.	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
6.2	Integrate consideration of the latest New York City projections of climate change and sea level rise (as published in <i>New York City Panel on Climate Change 2015 Report, Chapter 2: Sea Level Rise and Coastal Storms</i>) into the planning and design of projects in the city's Coastal Zone.	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
6.3	Direct public funding for flood prevention or erosion control measures to those locations where the investment will yield significant public benefit.	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
6.4	Protect and preserve non-renewable sources of sand for beach nourishment.	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
7	Minimize environmental degradation and negative impacts on public health from solid waste, toxic pollutants, hazardous materials, and industrial materials that may pose risks to the environment and public health and safety.	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
7.1	Manage solid waste material, hazardous wastes, toxic pollutants, substances hazardous to the environment, and the unenclosed storage of industrial materials to protect public health, control pollution and prevent degradation of coastal ecosystems.	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
7.2	Prevent and remediate discharge of petroleum products.	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
7.3	Transport solid waste and hazardous materials and site solid and hazardous waste facilities in a manner that minimizes potential degradation of coastal resources.	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
8	Provide public access to, from, and along New York City's coastal waters.	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
8.1	Preserve, protect, maintain, and enhance physical, visual and recreational access to the waterfront.	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
8.2	Incorporate public access into new public and private development where compatible with proposed land use and coastal location.	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
8.3	Provide visual access to the waterfront where physically practical.	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
8.4	Preserve and develop waterfront open space and recreation on publicly owned land at suitable locations.	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

		Promote	Hinder	N/A
8.5	Preserve the public interest in and use of lands and waters held in public trust by the State and City.	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
8.6	Design waterfront public spaces to encourage the waterfront's identity and encourage stewardship.	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
9	Protect scenic resources that contribute to the visual quality of the New York City coastal area.	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
9.1	Protect and improve visual quality associated with New York City's urban context and the historic and working waterfront.	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
9.2	Protect and enhance scenic values associated with natural resources.	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
10	Protect, preserve, and enhance resources significant to the historical, archaeological, architectural, and cultural legacy of the New York City coastal area.	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
10.1	Retain and preserve historic resources, and enhance resources significant to the coastal culture of New York City.	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
10.2	Protect and preserve archaeological resources and artifacts.	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

G. CERTIFICATION

The applicant or agent must certify that the proposed activity is consistent with New York City's approved Local Waterfront Revitalization Program, pursuant to New York State's Coastal Management Program. If this certification cannot be made, the proposed activity shall not be undertaken. If this certification can be made, complete this Section.

"The proposed activity complies with New York State's approved Coastal Management Program as expressed in New York City's approved Local Waterfront Revitalization Program, pursuant to New York State's Coastal Management Program, and will be conducted in a manner consistent with such program."

Applicant/Agent's Name: Nathan Gray, NYC Economic Development Corporation

Address: 110 William Street, NY, NY 10038

Telephone: 212-312-3718 Email: ngray@edc.nyc

Applicant/Agent's Signature: 

Date: April 26, 2017

Submission Requirements

For all actions requiring City Planning Commission approval, materials should be submitted to the Department of City Planning.

For local actions not requiring City Planning Commission review, the applicant or agent shall submit materials to the Lead Agency responsible for environmental review. A copy should also be sent to the Department of City Planning.

For State actions or funding, the Lead Agency responsible for environmental review should transmit its WRP consistency assessment to the Department of City Planning.

For Federal direct actions, funding, or permits applications, including Joint Applicants for Permits, the applicant or agent shall also submit a copy of this completed form along with his/her application to the [NYS Department of State Office of Planning and Development](#) and other relevant state and federal agencies. A copy of the application should be provided to the NYC Department of City Planning.

The Department of City Planning is also available for consultation and advisement regarding WRP consistency procedural matters.

New York City Department of City Planning

Waterfront and Open Space Division
120 Broadway, 31st Floor
New York, New York 10271
212-720-3525
wrp@planning.nyc.gov
www.nyc.gov/wrp

New York State Department of State

Office of Planning and Development
Suite 1010
One Commerce Place, 99 Washington Avenue
Albany, New York 12231-0001
(518) 474-6000
www.dos.ny.gov/opd/programs/consistency

Applicant Checklist

- Copy of original signed NYC Consistency Assessment Form
- Attachment with consistency assessment statements for all relevant policies
- For Joint Applications for Permits, one (1) copy of the complete application package
- Environmental Review documents
- Drawings (plans, sections, elevations), surveys, photographs, maps, or other information or materials which would support the certification of consistency and are not included in other documents submitted. All drawings should be clearly labeled and at a scale that is legible.

Appendix D

LPC Correspondence



AKRF, Inc.
Environmental, Planning, and Engineering Consultants
440 Park Avenue South
7th Floor
New York, NY 10016
tel: 212 696-0670
fax: 212 213-3191
www.akrf.com

March 27, 2017

Ms. Gina Santucci
New York City Landmarks Preservation Commission
1 Centre Street, 9th Floor
New York, NY 10007

Re: Downtown Far Rockaway Redevelopment Project Expanded Area

Dear Ms. Santucci:

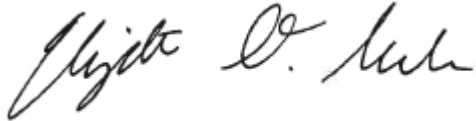
As you are aware, the City of New York, acting through the New York City Economic Development Corporation (EDC), is proposing a series of land use and other discretionary actions, including zoning map amendments, zoning text amendments, disposition and acquisition of property, establishment of an Urban Renewal Area (URA), and a special permit to implement recommendations of a comprehensive plan to redevelop and revitalize an approximately 21-block area of the Downtown Far Rockaway neighborhood of Queens. The NYC Office of the Deputy Mayor for Housing and Economic Development (ODMHED) is serving as lead agency for the environmental review pursuant to City Environmental Quality Review (CEQR) and the Uniform Land Use Review Procedure (ULURP).

In a comment letter dated June 14, 2016, LPC determined that the project site as then proposed did not possess archaeological significance. Since that time, two additional development sites are being considered for inclusion in the proposed rezoning area (see yellow-hatched areas in **Figure 1: Project Area Components**). The first site includes Block 15535, Lots 11 and 58 through 61, located on the northwestern side of Central Avenue near the terminus of Neilson Street. Lots 58 through 61 are currently developed with a 1-story commercial building and Lot 11 is occupied by a paved lot that is used for storage. The second site comprises Block 15574, Lot 36 and is located at the southwest corner of Gateway Boulevard and Mott Avenue. This lot is a vacant, fenced paved area that was once a private, publicly-accessible playground with landscaped areas and park benches.

At this time, we are requesting an initial assessment from your office regarding the potential archaeological sensitivity of the proposed project site as described above. Thank you for your

assistance with this matter. If you have any questions, please do not hesitate to contact me at emeade@akrf.com or by telephone at (646) 388-9811.

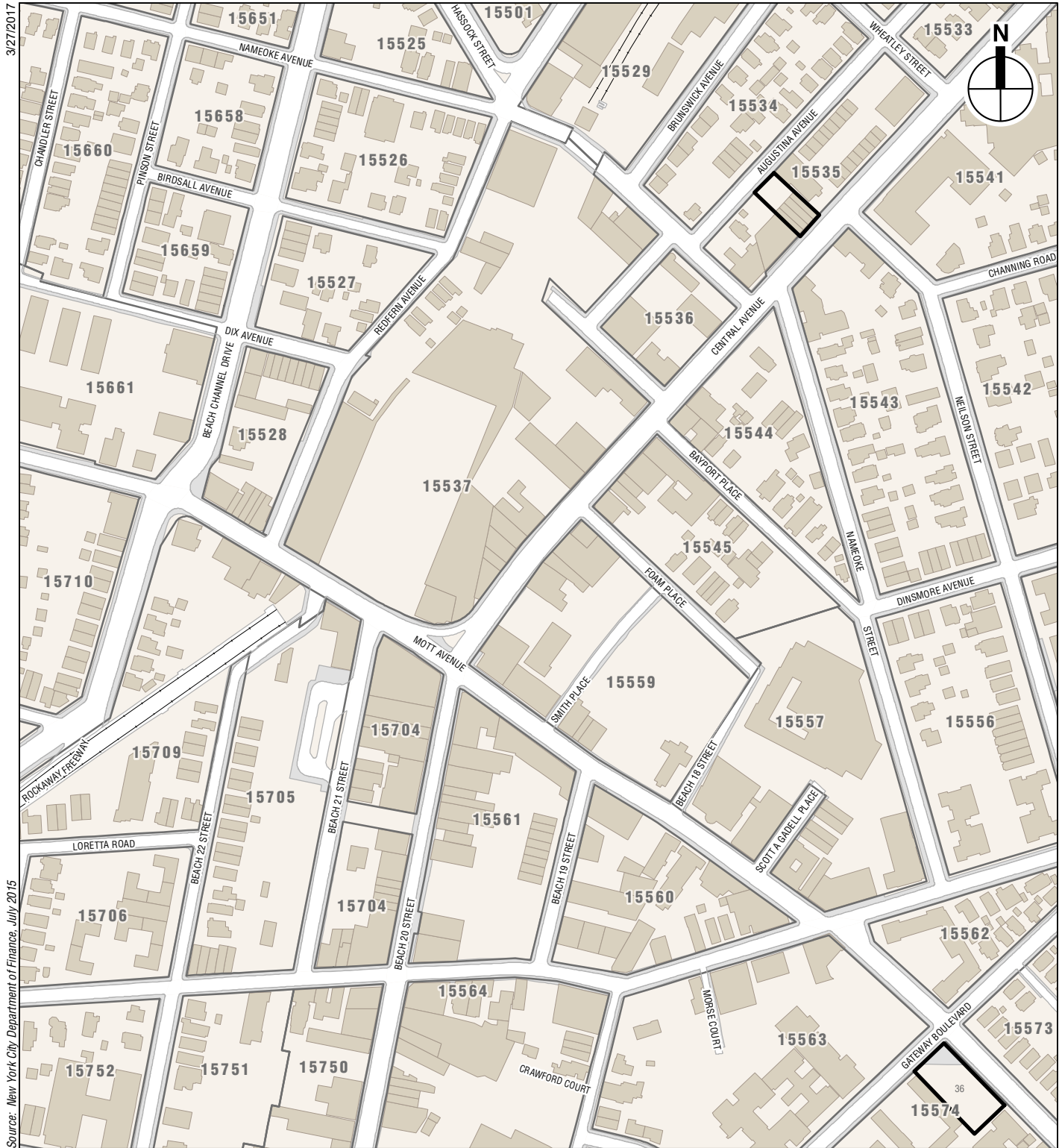
Sincerely,

A handwritten signature in black ink, appearing to read "Elizabeth D. Meade". The signature is written in a cursive, flowing style.

Elizabeth D. Meade, RPA
Technical Director/Archaeologist

cc:

Amanda Sutphin, New York City Landmarks Preservation Commission
Nathan Grey and Rebecca Gafvert, EDC
John Neill, AKRF



3/27/2017

Source: New York City Department of Finance, July 2015

 Additional Projected Development Sites

0 200 FEET

ENVIRONMENTAL REVIEW

Project number: DEPUTY MAYOR FINANCE/ECO DEV / 16DME010Q
Project: DOWNTOWN FAR ROCKAWAY REDEVELOPMENT PROJECT
Date received: 3/27/2017

Properties with no Archaeological significance:

- 72) ADDRESS: 13-25 AUGUSTINA AVENUE, BBL: 4155350011
- 73) ADDRESS: 13-20 CENTRAL AVENUE, BBL: 4155350058
- 74) ADDRESS: 13-22 CENTRAL AVENUE, BBL: 4155350059
- 75) ADDRESS: 13-24 CENTRAL AVENUE, BBL: 4155350060
- 76) ADDRESS: 13-26 CENTRAL AVENUE, BBL: 4155350061
- 77) ADDRESS: GATEWAY BOULEVARD, BBL: 4155740036

Gina Santucci

3/29/2017

SIGNATURE
Gina Santucci, Environmental Review Coordinator

DATE

File Name: 31550_FSO_DNP_03292017.doc

Appendix E

Travel Demand Forecast Tables

Traffic Level of Service Comparison Tables

Travel Demand Forecast - Site 9 (DEIS Program)

Land Use:	Residential		Community Center		Total	
	Size/Units:	135 DUs	6,000	gsf		
Peak Hour Trips:						
AM	110		12		122	
MD	56		26		82	
PM	120		14		134	
Sat MD	104		16		120	
Person Trips:						
AM	In	Out	In	Out	In	Out
Auto	6	37	1	1	7	38
Taxi	0	1	0	0	0	1
Subway	3	19	2	1	5	20
Rail	1	4	0	0	1	4
Bus	3	20	0	0	3	20
Walk/Other	2	14	4	3	6	17
Total	15	95	7	5	22	100
MD	In	Out	In	Out	In	Out
Auto	11	11	2	2	13	13
Taxi	0	0	0	0	0	0
Subway	6	6	3	3	9	9
Rail	1	1	0	0	1	1
Bus	6	6	1	1	7	7
Walk/Other	4	4	7	7	11	11
Total	28	28	13	13	41	41
PM	In	Out	In	Out	In	Out
Auto	33	14	1	2	34	16
Taxi	1	0	0	0	1	0
Subway	17	7	1	2	18	9
Rail	3	1	0	0	3	1
Bus	18	8	0	0	18	8
Walk/Other	13	5	2	6	15	11
Total	85	35	4	10	89	45
Sat MD	In	Out	In	Out	In	Out
Auto	21	21	1	1	22	22
Taxi	0	0	0	0	0	0
Subway	10	10	2	2	12	12
Rail	2	2	0	0	2	2
Bus	11	11	0	0	11	11
Walk/Other	8	8	5	5	13	13
Total	52	52	8	8	60	60
Vehicle Trips :						
AM	In	Out	In	Out	In	Out
Auto (Total)	5	34	1	1	6	35
Taxi	0	1	0	0	0	1
Taxi Balanced	1	1	0	0	1	1
Truck	0	0	0	0	0	0
Total	6	35	1	1	7	36
MD	In	Out	In	Out	In	Out
Auto (Total)	10	10	1	1	11	11
Taxi	0	0	0	0	0	0
Taxi Balanced	0	0	0	0	0	0
Truck	0	0	0	0	0	0
Total	10	10	1	1	11	11
PM	In	Out	In	Out	In	Out
Auto (Total)	30	13	1	1	31	14
Taxi	1	0	0	0	1	0
Taxi Balanced	1	1	0	0	1	1
Truck	0	0	0	0	0	0
Total	31	14	1	1	32	15
Sat MD	In	Out	In	Out	In	Out
Auto (Total)	19	19	1	1	20	20
Taxi	0	0	0	0	0	0
Taxi Balanced	0	0	0	0	0	0
Truck	0	0	0	0	0	0
Total	19	19	1	1	20	20
	<u>Total</u>					
AM	43					
MD	22					
PM	47					
Sat MD	40					

Travel Demand Forecast - Revised Site 9

Land Use:	Local Retail		Residential		Total	
	8,000	gsf	133	DUs		
Size/Units:						
Peak Hour Trips:						
AM	38		108		146	
MD	234		54		288	
PM	124		120		244	
Sat MD	144		104		248	
Person Trips:						
AM	In	Out	In	Out	In	Out
Auto	2	2	6	37	8	39
Taxi	0	0	0	1	0	1
Subway	2	2	3	18	5	20
Rail	0	0	1	4	1	4
Bus	1	1	3	19	4	20
Walk/Other	14	14	2	14	16	28
Total	19	19	15	93	34	112
MD	In	Out	In	Out	In	Out
Auto	13	13	11	11	24	24
Taxi	0	0	0	0	0	0
Subway	11	11	5	5	16	16
Rail	0	0	1	1	1	1
Bus	6	6	6	6	12	12
Walk/Other	87	87	4	4	91	91
Total	117	117	27	27	144	144
PM	In	Out	In	Out	In	Out
Auto	7	7	32	14	39	21
Taxi	0	0	1	0	1	0
Subway	6	6	17	7	23	13
Rail	0	0	3	1	3	1
Bus	3	3	19	8	22	11
Walk/Other	46	46	13	5	59	51
Total	62	62	85	35	147	97
Sat MD	In	Out	In	Out	In	Out
Auto	6	5	21	21	27	26
Taxi	0	0	0	0	0	0
Subway	6	5	10	10	16	15
Rail	0	0	2	2	2	2
Bus	4	3	11	11	15	14
Walk/Other	63	52	8	8	71	60
Total	79	65	52	52	131	117
Vehicle Trips :						
AM	In	Out	In	Out	In	Out
Auto (Total)	1	1	5	34	6	35
Taxi	0	0	0	1	0	1
Taxi Balanced	0	0	1	1	1	1
Truck	0	0	0	0	0	0
Total	1	1	6	35	7	36
MD	In	Out	In	Out	In	Out
Auto (Total)	7	7	10	10	17	17
Taxi	0	0	0	0	0	0
Taxi Balanced	0	0	0	0	0	0
Truck	0	0	0	0	0	0
Total	7	7	10	10	17	17
PM	In	Out	In	Out	In	Out
Auto (Total)	4	4	29	13	33	17
Taxi	0	0	1	0	1	0
Taxi Balanced	0	0	1	1	1	1
Truck	0	0	0	0	0	0
Total	4	4	30	14	34	18
Sat MD	In	Out	In	Out	In	Out
Auto (Total)	3	3	19	19	22	22
Taxi	0	0	0	0	0	0
Taxi Balanced	0	0	0	0	0	0
Truck	0	0	0	0	0	0
Total	3	3	19	19	22	22
Total Vehicle Trips						
	<u>In</u>	<u>Out</u>	<u>Total</u>			
AM	7	36	43			
MD	17	17	34			
PM	34	18	52			
Sat MD	22	22	44			

Notes:

25% linked-trip credit applied to local retail use.

A-Application Additional Trips - Site 9

Land Use:	Total	
Size/Units:		
Peak Hour Trips:		
AM	24	
MD	206	
PM	110	
Sat MD	128	
Person Trips:		
AM	In	Out
Auto	1	1
Taxi	0	0
Subway	0	0
Rail	0	0
Bus	1	0
Walk/Other	10	11
Total	12	12
MD	In	Out
Auto	11	11
Taxi	0	0
Subway	7	7
Rail	0	0
Bus	5	5
Walk/Other	80	80
Total	103	103
PM	In	Out
Auto	5	5
Taxi	0	0
Subway	5	4
Rail	0	0
Bus	4	3
Walk/Other	44	40
Total	58	52
Sat MD	In	Out
Auto	5	4
Taxi	0	0
Subway	4	3
Rail	0	0
Bus	4	3
Walk/Other	58	47
Total	71	57
Vehicle Trips :		
AM	In	Out
Auto (Total)	0	0
Taxi	0	0
Taxi Balanced	0	0
Truck	0	0
Total	0	0
MD	In	Out
Auto (Total)	6	6
Taxi	0	0
Taxi Balanced	0	0
Truck	0	0
Total	6	6
PM	In	Out
Auto (Total)	3	3
Taxi	0	0
Taxi Balanced	0	0
Truck	0	0
Total	3	3
Sat MD	In	Out
Auto (Total)	2	2
Taxi	0	0
Taxi Balanced	0	0
Truck	0	0
Total	2	2

Travel Demand Forecast - Site 18

Land Use:	Residential		Total	
Size/Units:	71	DUs		
Peak Hour Trips:				
AM	58		58	
MD	30		30	
PM	64		64	
Sat MD	56		56	
Person Trips:				
AM	In	Out	In	Out
Auto	4	20	4	20
Taxi	0	0	0	0
Subway	2	10	2	10
Rail	0	2	0	2
Bus	2	10	2	10
Walk/Other	1	7	1	7
Total	9	49	9	49
MD	In	Out	In	Out
Auto	6	6	6	6
Taxi	0	0	0	0
Subway	3	3	3	3
Rail	1	1	1	1
Bus	3	3	3	3
Walk/Other	2	2	2	2
Total	15	15	15	15
PM	In	Out	In	Out
Auto	17	8	17	8
Taxi	0	0	0	0
Subway	9	4	9	4
Rail	2	1	2	1
Bus	9	4	9	4
Walk/Other	7	3	7	3
Total	44	20	44	20
Sat MD	In	Out	In	Out
Auto	11	11	11	11
Taxi	0	0	0	0
Subway	6	6	6	6
Rail	1	1	1	1
Bus	6	6	6	6
Walk/Other	4	4	4	4
Total	28	28	28	28
Vehicle Trips :				
AM	In	Out	In	Out
Auto (Total)	4	18	4	18
Taxi	0	0	0	0
Taxi Balanced	0	0	0	0
Truck	0	0	0	0
Total	4	18	4	18
MD	In	Out	In	Out
Auto (Total)	5	5	5	5
Taxi	0	0	0	0
Taxi Balanced	0	0	0	0
Truck	0	0	0	0
Total	5	5	5	5
PM	In	Out	In	Out
Auto (Total)	15	7	15	7
Taxi	0	0	0	0
Taxi Balanced	0	0	0	0
Truck	0	0	0	0
Total	15	7	15	7
Sat MD	In	Out	In	Out
Auto (Total)	10	10	10	10
Taxi	0	0	0	0
Taxi Balanced	0	0	0	0
Truck	0	0	0	0
Total	10	10	10	10

Travel Demand Forecast - Site 19

Land Use:	Residential		Total	
Size/Units:	27	DUs		
Peak Hour Trips:				
AM	22		22	
MD	12		12	
PM	24		24	
Sat MD	22		22	
Person Trips:				
AM	In	Out	In	Out
Auto	1	7	1	7
Taxi	0	0	0	0
Subway	1	4	1	4
Rail	0	1	0	1
Bus	1	4	1	4
Walk/Other	0	3	0	3
Total	3	19	3	19
MD	In	Out	In	Out
Auto	3	3	3	3
Taxi	0	0	0	0
Subway	1	1	1	1
Rail	0	0	0	0
Bus	1	1	1	1
Walk/Other	1	1	1	1
Total	6	6	6	6
PM	In	Out	In	Out
Auto	6	3	6	3
Taxi	0	0	0	0
Subway	3	1	3	1
Rail	1	0	1	0
Bus	4	2	4	2
Walk/Other	3	1	3	1
Total	17	7	17	7
Sat MD	In	Out	In	Out
Auto	5	5	5	5
Taxi	0	0	0	0
Subway	2	2	2	2
Rail	0	0	0	0
Bus	2	2	2	2
Walk/Other	2	2	2	2
Total	11	11	11	11
Vehicle Trips :				
AM	In	Out	In	Out
Auto (Total)	1	6	1	6
Taxi	0	0	0	0
Taxi Balanced	0	0	0	0
Truck	0	0	0	0
Total	1	6	1	6
MD	In	Out	In	Out
Auto (Total)	3	3	3	3
Taxi	0	0	0	0
Taxi Balanced	0	0	0	0
Truck	0	0	0	0
Total	3	3	3	3
PM	In	Out	In	Out
Auto (Total)	5	3	5	3
Taxi	0	0	0	0
Taxi Balanced	0	0	0	0
Truck	0	0	0	0
Total	5	3	5	3
Sat MD	In	Out	In	Out
Auto (Total)	5	5	5	5
Taxi	0	0	0	0
Taxi Balanced	0	0	0	0
Truck	0	0	0	0
Total	5	5	5	5

A-Application Additional Trips - Total

Land Use:	Total	
Size/Units:		
Peak Hour Trips:		
AM	104	
MD	248	
PM	198	
Sat MD	206	
Person Trips:		
AM	In	Out
Auto	6	28
Taxi	0	0
Subway	3	14
Rail	0	3
Bus	4	14
Walk/Other	11	21
Total	24	80
MD	In	Out
Auto	20	20
Taxi	0	0
Subway	11	11
Rail	1	1
Bus	9	9
Walk/Other	83	83
Total	124	124
PM	In	Out
Auto	28	16
Taxi	0	0
Subway	17	9
Rail	3	1
Bus	17	9
Walk/Other	54	44
Total	119	79
Sat MD	In	Out
Auto	21	20
Taxi	0	0
Subway	12	11
Rail	1	1
Bus	12	11
Walk/Other	64	53
Total	110	96
Vehicle Trips :		
AM	In	Out
Auto (Total)	5	24
Taxi	0	0
Taxi Balanced	0	0
Truck	0	0
Total	5	24
MD	In	Out
Auto (Total)	14	14
Taxi	0	0
Taxi Balanced	0	0
Truck	0	0
Total	14	14
PM	In	Out
Auto (Total)	23	13
Taxi	0	0
Taxi Balanced	0	0
Truck	0	0
Total	23	13
Sat MD	In	Out
Auto (Total)	17	17
Taxi	0	0
Taxi Balanced	0	0
Truck	0	0
Total	17	17

Traffic LOS: No-Action vs. With-Action vs. Amended RWCDs

	LANE GROUP	WEEKDAY AM PEAK HOUR									WEEKDAY MD PEAK HOUR									WEEKDAY PM PEAK HOUR									SAT PEAK HOUR								
		No-Action			With-Action			Amended RWCDs			No-Action			With-Action			Amended RWCDs			No-Action			With-Action			Amended RWCDs			No-Action			With-Action			Amended RWCDs		
		V/C	Delay	LOS	V/C	Delay	LOS	V/C	Delay	LOS	V/C	Delay	LOS	V/C	Delay	LOS	V/C	Delay	LOS	V/C	Delay	LOS	V/C	Delay	LOS	V/C	Delay	LOS	V/C	Delay	LOS	V/C	Delay	LOS			
4. Cornaga Avenue (E-W) @ Rockaway Freeway (N-S)	EB - LTR	0.38	22.0	C	0.38	22.1	C	0.38	22.1	C	0.24	20.0	B	0.25	20.2	C	0.26	20.2	C	0.25	20.1	C	0.26	20.2	C	0.26	20.3	C	0.16	19.0	B	0.17	19.1	B	0.17	19.1	B
	WB - LTR	0.62	29.7	C	0.67	31.8	C	0.67	31.8	C	0.49	25.6	C	0.54	27.0	C	0.55	27.2	C	0.79	39.6	D	0.85	45.9	D *	0.85	46.9	D *	0.45	24.1	C	0.49	25.0	C	0.49	25.1	C
	NB - TR	0.39	25.7	C	0.41	26.1	C	0.41	26.1	C	0.61	30.7	C	0.68	33.3	C	0.68	33.5	C	0.44	26.5	C	0.53	28.3	C	0.53	28.5	C	0.34	24.8	C	0.40	25.7	C	0.40	25.8	C
	SB - L	0.07	36.6	D	0.07	36.6	D	0.07	36.6	D	0.12	37.6	D	0.12	37.6	D	0.12	37.6	D	0.07	36.7	D	0.07	36.7	D	0.07	36.7	D	0.11	37.3	D	0.11	37.3	D	0.11	37.3	D
	SB - TR	0.15	13.1	B	0.20	13.6	B	0.20	13.6	B	0.16	13.2	B	0.18	13.5	B	0.18	13.5	B	0.18	13.3	B	0.20	13.6	B	0.20	13.6	B	0.15	13.1	B	0.18	13.4	B	0.18	13.4	B
5. Cornaga Avenue (E-W) @ Beach 22nd Street (N-S)	EB - TR	0.40	10.4	B	0.41	10.5	B	0.41	10.5	B	0.29	9.2	A	0.31	9.4	A	0.32	9.5	A	0.29	9.2	A	0.32	9.4	A	0.32	9.5	A	0.24	8.7	A	0.26	8.8	A	0.26	8.8	A
	WB - LT	0.35	10.3	B	0.37	10.4	B	0.37	10.5	B	0.29	9.4	A	0.31	9.5	A	0.31	9.6	A	0.39	10.5	B	0.40	10.7	B	0.40	10.7	B	0.20	8.5	A	0.21	8.6	A	0.21	8.6	A
	NB - LR	0.72	31.5	C	0.80	38.1	D	0.80	38.1	D	0.60	25.4	C	0.88	52.1	D *	0.88	52.1	D *	0.75	34.9	C	0.98	71.8	E *	0.98	71.8	E *	0.56	24.1	C	0.73	34.1	C	0.73	34.1	C
	SB - LTR	0.41	18.8	B	0.49	20.5	C	0.49	20.5	C	0.44	19.4	B	0.64	25.0	C	0.64	25.0	C	0.66	25.2	C	0.81	35.2	D	0.81	35.2	D	0.48	20.2	C	0.60	23.4	C	0.60	23.4	C
	SB - TR	0.41	18.8	B	0.49	20.5	C	0.49	20.5	C	0.44	19.4	B	0.64	25.0	C	0.64	25.0	C	0.66	25.2	C	0.81	35.2	D	0.81	35.2	D	0.48	20.2	C	0.60	23.4	C	0.60	23.4	C
6. Cornaga Avenue (E-W) @ Beach 20th Street (SB)	EB - TR	0.34	9.7	A	0.35	9.8	A	0.35	9.8	A	0.28	9.1	A	0.31	9.4	A	0.31	9.5	A	0.27	9.0	A	0.29	9.2	A	0.29	9.2	A	0.19	8.3	A	0.20	8.5	A	0.21	8.5	A
	WB - LT	0.46	11.4	B	0.51	12.3	B	0.51	12.4	B	0.40	10.7	B	0.48	11.9	B	0.49	12.1	B	0.43	11.0	B	0.50	12.1	B	0.50	12.2	B	0.28	9.3	A	0.34	10.0	A	0.35	10.0	B
	SB - LTR	1.16	115.7	F	1.31	178.0	F *	1.31	178.0	F *	1.31	178.7	F	1.66	330.7	F *	1.66	331.9	F *	1.39	211.7	F	1.98	470.4	F *	1.98	471.6	F *	1.45	238.0	F	1.84	408.4	F *	1.84	408.4	F *
7. Mott Avenue (E-W) @ Cornaga Avenue (N-S)	EB - LTR	0.59	36.9	D	0.85	59.6	E *	0.86	60.2	E *	0.37	29.4	C	0.56	35.8	D	0.56	35.8	D	0.53	33.9	C	0.71	43.2	D	0.71	43.4	D	0.52	33.8	C	0.69	42.5	D	0.69	42.5	D
	WB - LTR	0.36	28.8	C	0.42	30.0	C	0.43	30.2	C	0.25	26.8	C	0.34	28.4	C	0.36	28.9	C	0.37	29.1	C	0.51	32.3	C	0.52	32.7	C	0.21	26.2	C	0.31	27.9	C	0.32	28.1	C
	NB - LTR	0.71	40.9	D	0.75	43.4	D	0.75	43.4	D	0.74	42.9	D	0.80	48.0	D *	0.82	50.5	D *	0.77	46.1	D	0.83	52.7	D *	0.85	55.3	E *	0.81	48.3	D	0.87	56.0	E *	0.88	58.1	E *
	SB - LTR	0.94	69.3	E	1.01	88.6	F *	1.06	102.1	F *	0.92	66.5	E	1.12	124.5	F *	1.21	157.5	F *	0.94	70.2	E	1.29	186.4	F *	1.43	244.5	F *	0.68	40.3	D	0.88	60.9	E *	0.99	85.0	F *
	SB - TR	0.94	69.3	E	1.01	88.6	F *	1.06	102.1	F *	0.92	66.5	E	1.12	124.5	F *	1.21	157.5	F *	0.94	70.2	E	1.29	186.4	F *	1.43	244.5	F *	0.68	40.3	D	0.88	60.9	E *	0.99	85.0	F *
8. Cornaga Avenue (E-W) @ Beach 9th Street / Empire Avenue (N-S)	EB - LTR	1.03	72.9	E	1.20	134.3	F *	1.24	149.0	F *	0.83	38.8	D	0.94	53.5	D *	0.95	56.2	E *	0.94	51.5	D	1.05	79.4	E *	1.07	84.6	F *	0.71	30.1	C	0.84	39.2	D	0.86	41.7	D
	WB - LTR	0.27	19.9	B	0.28	20.1	C	0.28	20.1	C	0.15	17.9	B	0.17	18.2	B	0.17	18.2	B	0.24	19.3	B	0.27	19.7	B	0.27	19.7	B	0.06	16.8	B	0.07	17.0	B	0.07	17.0	B
	NB - LTR	0.88	36.7	D	0.89	55.7	E *	0.89	56.6	E *	0.76	42.0	D	0.81	46.6	D	0.82	47.8	D *	0.75	40.2	D	0.79	44.0	D	0.80	45.3	D	0.36	25.6	C	0.40	26.6	C	0.40	26.8	C
	SB - LTR	1.02	76.4	E	1.04	82.3	F *	1.06	86.0	F *	0.80	39.7	D	0.86	45.4	D *	0.88	47.2	D *	1.10	101.2	F	1.22	145.5	F *	1.25	158.4	F *	0.55	28.8	C	0.66	32.7	C	0.69	33.8	C
	SB - TR	1.02	76.4	E	1.04	82.3	F *	1.06	86.0	F *	0.80	39.7	D	0.86	45.4	D *	0.88	47.2	D *	1.10	101.2	F	1.22	145.5	F *	1.25	158.4	F *	0.55	28.8	C	0.66	32.7	C	0.69	33.8	C
9. Mott Avenue (E-W) @ Beach Channel Drive (N-S)	EB - LTR	1.15	125.8	F	1.17	133.2	F *	1.17	133.2	F *	0.49	25.4	C	0.62	30.3	C	0.62	30.4	C	1.14	123.2	F	1.19	139.8	F *	1.19	139.8	F *	0.84	47.4	D	0.91	57.5	E *	0.91	57.5	E *
	WB - LT	0.72	37.6	D	0.77	43.6	D	0.77	43.6	D	0.53	27.6	C	0.62	32.5	C	0.62	32.5	C	0.79	39.4	D	0.75	37.6	D	0.75	37.6	D	0.51	26.3	C	0.49	26.4	C	0.49	26.4	C
	WB - R	0.15	10.9	B	0.16	11.0	B	0.16	11.0	B	0.21	11.4	B	0.30	13.2	B	0.30	13.2	B	0.19	11.3	B	0.23	12.2	B	0.24	12.2	B	0.23	11.9	B	0.30	13.4	B	0.30	13.4	B
	NB - L	0.42	33.9	C	0.57	50.8	D *	0.57	50.8	D *	0.47	30.5	C	0.60	39.5	D	0.60	39.5	D	0.16	23.0	C	0.25	28.5	C	0.25	28.5	C	0.08	20.8	C	0.12	22.3	C	0.12	22.3	C
	NB - T	0.93	54.0	D	0.96	59.0	E	0.96	59.0	E	0.89	48.2	D	0.94	55.1	E *	0.94	55.1	E *	0.82	39.5	D	1.29	180.4	F *	1.29	180.4	F *	0.81	38.7	D	0.85	42.1	D	0.85	42.1	D
	NB - R	0.18	21.5	C	0.21	22.0	C	0.21	22.0	C	0.21	21.9	C	0.28	22.9	C	0.28	22.9	C	0.29	23.2	C	0.42	25.6	C	0.42	25.6	C	0.19	21.6	C	0.27	22.9	C	0.27	22.9	C
	SB - L	0.75	34.2	C	1.11	108.1	F *	1.11	108.1	F *	0.80	36.0	D	1.35	205.6	F *	1.35	205.6	F *	0.65	25.8	C	1.23	160.3	F *	1.23	160.3	F *	0.69	27.9	C	1.17	134.1	F *	1.17	134.1	F *
	SB - TR	0.92	38.6	D	1.06	73.1	E *	1.06	73.1	E *	0.82	28.5	C	0.93	39.8	D	0.93	39.8	D	0.97	47.6	D	1.11	88.8	F *	1.11	88.8	F *	0.88	32.8	C	1.01	57.0	E *	1.01	57.0	E *
	SB - TR	0.92	38.6	D	1.06	73.1	E *	1.06	73.1	E *	0.82	28.5	C	0.93	39.8	D	0.93	39.8	D	0.97	47.6	D	1.11	88.8	F *	1.11	88.8	F *	0.88	32.8	C	1.01	57.0	E *	1.01	57.0	E *
	SB - TR	0.92	38.6	D	1.06	73.1	E *	1.06	73.1	E *	0.82	28.5	C	0.93	39.8	D	0.93	39.8	D	0.97	47.6	D	1.11	88.8	F *	1.11	88.8	F *	0.88	32.8	C	1.01	57.0	E *	1.01	57.0	E *
10. Mott Avenue (E-W) @ Beach 21st Street (N-S)	EB - LT	0.21	6.1	A	0.19	6.0	A	0.19	6.0	A	0.16	5.8	A	0.12	5.6	A	0.12	5.6	A	0.22	6.1	A	0.18	5.9	A	0.18	5.9	A	0.20	6.0	A	0.15	5.7	A	0.15	5.7	A
	WB - TR	0.42	8.2	A	0.43	8.4	A	0.43	8.4	A	0.40	8.0	A	0.42	8.2	A	0.42	8.2	A	0.49	9.1	A	0.54	9.8	A	0.54	9.8	A	0.34	7.4	A	0.37	7.7	A	0.37	7.7	A
	NB - LT	0.66	44.1	D	1.29	208.8	F *	1.29	208.8	F *	0.81	54.8	D																								

Mott Avenue & Cornaga Avenue: Action-with-Mitigation (DEIS) vs. Amended Action-with-Mitigation (Amended RWCDs)

Intersection	LANE GROUP	No-Action			DEIS						Amended RWCDs						Proposed Mitigation Measures	
					With-Action		Action-with-Mitigation		Proposed Mitigation Measures	Amended With-Action			Amended Action-with-Mitigation					
		V/C RATIO	Delay (sec.)	LOS	V/C RATIO	Delay (sec.)	V/C RATIO	Delay (sec.)		V/C RATIO	Delay (sec.)	LOS	V/C RATIO	Delay (sec.)	LOS			
WEEKDAY AM PEAK HOUR																		
7. Mott Avenue (E-W) @ Cornaga Avenue (N-S)	EB - LTR	0.59	36.9	D	0.85	59.6	E *	0.73	43.9	D	- Install "No Standing Anytime" regulation along south curb of EB approach M-F 7AM-10AM - Shift 1s of green time from pedestrian phase to N/S phase	0.86	60.2	E *	0.73	44.1	D	- Install "No Standing Anytime" regulation along south curb of EB approach M-F 7AM-10AM - Shift 2 s of green time from pedestrian phase to N/S phase
	WB - LT	0.36	28.8	C	0.42	30.0	C	0.42	30.0	C		0.43	30.2	C	0.43	30.2	C	
	NB - LTR	0.71	40.9	D	0.75	43.4	D	0.71	40.3	D		0.75	43.4	D	0.71	40.3	D	
	SB - LTR	0.94	69.3	E	1.01	88.6	F *	0.96	72.5	E		1.06	102.1	F *	1.00	68.3	E	
	WEEKDAY MIDDAY PEAK HOUR																	
	EB - LTR	0.37	29.4	C	0.56	35.8	D	0.63	42.1	D	- Shift 3s of green time from E/W phase to N/S phase	0.56	35.8	D	0.63	42.3	D	- Shift 3s of green time from E/W phase to N/S phase
	WB - LT	0.25	26.8	C	0.34	28.4	C	0.38	31.8	C		0.36	28.9	C	0.41	32.4	C	
	NB - LTR	0.74	42.9	D	0.80	48.0	D *	0.71	38.2	D		0.82	50.5	D *	0.73	39.3	D	
	SB - LTR	0.92	66.5	E	1.12	124.5	F *	0.94	67.0	E		1.21	157.5	F *	1.00	82.9	F *	
	WEEKDAY PM PEAK HOUR																	
	EB - LTR	0.53	33.9	C	0.71	43.2	D	0.71	43.2	D	- Shift 1s of green time from pedestrian phase to N/S phase	0.71	43.4	D	0.71	43.4	D	- Shift 1s of green time from pedestrian phase to N/S phase
	WB - LT	0.37	29.1	C	0.51	32.3	C	0.51	32.3	C		0.52	32.7	C	0.52	32.7	C	
NB - LTR	0.77	46.1	D	0.83	52.7	D *	0.79	46.8	D	0.85		55.3	E *	0.80	48.7	D		
SB - LTR	0.94	70.2	E	1.29	186.4	F *	1.21	155.3	F *	1.43		244.5	F *	1.34	208.5	F *		
WEEKDAY SAT PEAK HOUR																		
EB - LTR	0.52	33.8	C	0.69	42.5	D	0.66	42.4	D	- Shift 3s of green time from E/W phase to N/S phase	0.69	42.5	D	0.68	43.2	D	- Shift 3s of green time from E/W phase to N/S phase	
WB - LT	0.21	26.2	C	0.31	27.9	C	0.33	30.8	C		0.32	28.1	C	0.36	31.4	C		
NB - LTR	0.81	48.3	D	0.87	56.0	E *	0.75	40.3	D		0.88	58.1	E *	0.76	41.4	D		
SB - LTR	0.68	40.3	D	0.88	60.9	E *	0.75	42.1	D		0.99	85.0	F *	0.84	51.0	D *		

Notes:
 EB-Eastbound, WB-Westbound, NB-Northbound, SB-Southbound
 L-Left, T-Through, R-Right, DefL-Analysis considers a defacto left lane on this approach
 V/C Ratio - Volume to Capacity Ratio, sec. - Seconds
 LOS - Level of Service
 Shading denotes lane groups with unmitigated impacts
 Analysis is based on the 2000 Highway Capacity Manual methodology (HCS+, version 5.5)